



Mixed Use Center **South**

City of San Pablo

community vision + regulating code + implementation

January, 2012

To: Ms. Tina Gallegos
City Planner
City of San Pablo

From: Mr. Ian Ross, Principal
City Design Collective

Re: Development Code for the Mixed Use Center South

Dear Ms. Gallegos,

On behalf of the City Design Collective, I am pleased to present to you the Development Code for the Mixed Use Center South. In support of the community's desire to attract new investment, this code's land use regulations allow for flexibility to attract and enable a wide range of new development types. To ensure that all new development supports the envisioned Redevelopment Masterplan, regulations that govern form are customized in support of the community's desire for walkable streets and attractive public open spaces.

Best regards,



Ian Wolfe Ross
Principal
City Design Collective

City of San Pablo

DEVELOPMENT CODE
for the

Mixed-Use Center **South**

JANUARY, 2012

City Council / EDC Board

Paul V. Morris, Mayor / Chair
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Nell Trundle, Commissioner

Mixed Use Center South Subcommittee

Paul V. Morris, Mayor / Chair
Leonard R. McNeil, Councilmember / Director
Matt Rodriguez, City Manager / Executive Director
Kelsey D. Worthy, Assistant City Manager / Assistant Executive Director
Sonia Rivas, Redevelopment Analyst / Project Manager
Tina Gallegos, City Planner

City Staff / EDC Staff

Matt Rodriguez, City Manager / Executive Director
Kelsey D. Worthy, Assistant City Manager / Assistant Executive Director
Sonia Rivas, Redevelopment Analyst / Project Manager
Tina Gallegos, City Planner

Consultant Team



community vision + urban design

EMC Planning Group

ceqa analysis

Special thanks are offered to members of the San Pablo community who offered invaluable input to the public planning process.



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- 3.3 Administration (tbd city)
- 3.4 Finance (tbd city)

Section 1: Introduction

1.1 Orientation

1.2 Community Vision

1.1 Orientation

1.1.1 Purpose

The City of San Pablo has made great strides to envision and plan for new investment throughout the San Pablo Avenue Corridor. In recent years, the City has revisited city-wide and site-specific planning policies, has analyzed economic conditions, and has engaged in a comprehensive community planning process to envision desired growth and change.

Throughout this process, the community identified the Mixed Use Center South (former Circle-S site) as a key location on which to establish a highly attractive and walkable mixed-use destination containing a blend of residential, office, civic and cultural uses that attract residents, workers, and visitors.

The purpose of the Development Code is to translate the community's vision for the site into form-based development regulations customized to enable and guide desired transformation in support of long term economic and environmental health.

In doing so, this Code employs land use and site development regulations to attract a wide-range of new investment, while ensuring that all new investment contributes to fulfilling the community's vision for a vibrant, walkable, mixed-use town center.

1.1.2 Approach

Overview

The Regulating Code (code) calls for a significant departure from conventional planning and zoning practice. Rather than limiting each parcel to a single land use, the code provides flexibility with regard to land use, parking, and site development regulations to enable a range of possible outcomes.

Site Plan and Architectural Review

Proposed construction of buildings and related facilities shall require site plan and architectural design review as established by the City. City design review will address building placement and layout, architecture, landscape and open space design, parking location and design, signage location, and design of proposed conditions affecting the function and visual appearance of the project and its surroundings.

Operating Principles

The following operating principles recognize that new investment may occur incrementally:

1. The Regulating Code is not static. As the Plan Area develops over time, so too may the market forces and consumer preferences that affect development feasibility. On-going evaluation of the land use and site development regulations is needed to ensure long-term success.
2. Redevelopment is most successful when public and private investment are coordinated and directed towards achieving a common goal. The City is dedicated to assisting new development, and will continue to invest in capital improvements as funding allows to catalyze and support the envisioned transformation.

1.1.3 Applicability

Relationship to the General Plan

The City of San Pablo adopts this Code as a primary tool to carry out the goals, objectives, and policies of the San Pablo General Plan. The regulations herein are consistent with the General Plan, and any land use or development approved in compliance with this Code will also be consistent with the General Plan

Relationship to the San Pablo Avenue Specific Plan

This Regulating Plan supplements and in some cases replaces the requirements of the San Pablo Avenue Specific Plan.

1. When the content of this Plan conflicts with the San Pablo Avenue Specific Plan, this Plan shall govern.

Relationship to the City's Municipal Code

Applicable requirements of the Municipal Code addressing topics not covered by this Code shall apply to site development and land uses in the Plan Area.

The standards of the San Pablo Municipal Code which address topics of development and land use regulation not covered by this Plan remain applicable to development within the Plan Area.

1. When the content of this Plan conflicts with the San Pablo Municipal Code, this Code shall govern.
2. Requirements of the San Pablo Municipal Code addressing topics not covered by this Code including but not limited to the California and San Pablo Building Codes shall apply.

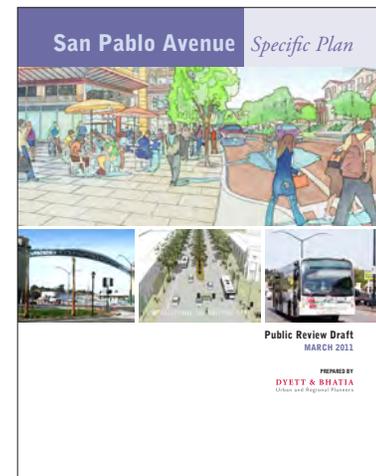
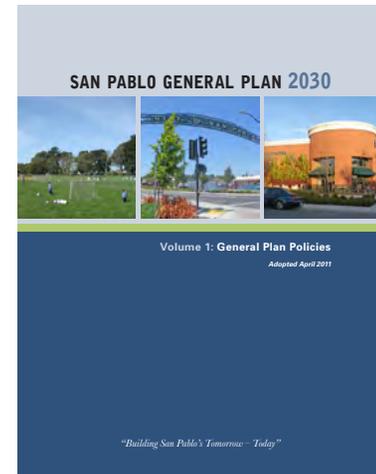
Code Exceptions

The Plan Area contains a number of unique conditions related to property configuration, parcel size, adjacencies and access. These conditions may create design and construction challenges that require flexibility in the application of the Code's development regulations. It is therefore sometimes in the public interest to make an exception to regulations contained in the Regulating Code.

Generally, the Planning Commission may authorize minor adjustments to a development proposal not specified by or approved by this Plan. Significant deviations from this Plan as related to site development shall be granted only after application and approval by the City. The City may require that the application for a major exception be considered at a public hearing before the Planning Commission and/or City Council.

An exception may be approved only where the Planning Commission or City Council makes the following findings:

1. The applicant demonstrates that a clearly superior design solution would result if the exception were granted, and the new solution is supportive of the intent of this Code's applicable regulations.
2. The proposed exception will not result in conditions that adversely affect public health and/or safety.
3. The proposed exception will not detract from the peaceful enjoyment of residents or visitors.



1.1 Orientation

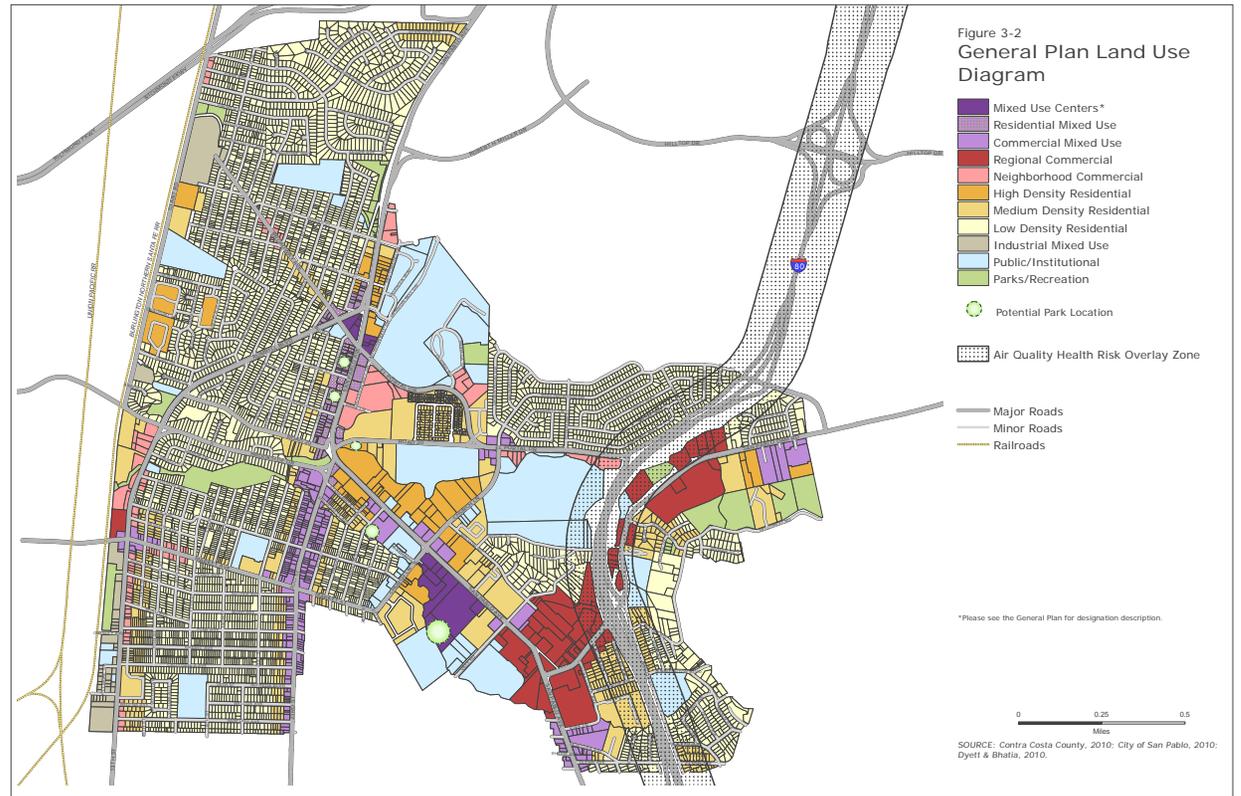
1.1.4 Plan Area

The Mixed Use Center South comprises approximately 16 acres of land located southwest of San Pablo Avenue, southeast of Church Lane. The southeast border is located approximately 200 feet northwest of Vale Road.

Having over 1,250 feet of frontage along San Pablo Avenue and located less than one half mile from Highway 80, the site presents an extraordinary opportunity to locate significant new investment where it is visible to over twenty thousand daily passers by and easily accessible to well over 250 thousand daily commuters.



Plan Area, Location, Access, and Context.



General Plan Land Use Diagram

1.1.5 Development Framework

General Plan Designation: *Mixed Use Center South*
 “The Mixed Use Center South designation describes a new high-intensity mixed-use destination in an area with high growth potential. The Mixed Use Center South designation aims to establish a major new activity center and new citywide and regional destination for the City of San Pablo. Uses may include commercial, office (including medical offices), residential, institutional, and hotel. Typical heights are expected to reach three to five stories, with a maximum height of 60 feet. The maximum FAR is 2.5 and the maximum residential density allowed is 60 units per gross acre (included within the FAR limit).”
 -*San Pablo Avenue Specific Plan*

Specific Plan Guiding Policies:

- 4-G-1 Develop the Circle-S site with a vital mix of synergistic retail, eating/ drinking establishments, office, institutional, and residential uses, and as a citywide and regional destination.
- 4-G-2 Ensure that streets and open spaces form the framework for development, with the public realm designed to be accessible from the surroundings, and designed to enhance pedestrian movement.
- 4-G-3 Encourage a mix of uses that is compatible with and caters to the needs of the community and region.

Economics

The market analysis performed for the San Pablo Avenue Specific Plan establishes ranges of possible investment for the Specific Plan Study area in entirety as detailed below.

**Market Study Results Summary
 San Pablo Corridor Market Study Review**

Development Type	Potential Incremental Demand	
	2015	2030
Residential (units)	390	900-1,300
Retail (square feet)		150,000-300,000
Office (square feet)	30,000	122,100

Source: Dyett & Bhatia

	EXISTING	FOCUS AREAS					NEW DEVELOPMENT		TOTAL
		CIRCLE-S	MISSION PLAZA	WEST SAN PABLO AVENUE	TOWNE CENTER	SOUTH SAN PABLO AVENUE	OTHER AREAS	TOTAL NEW	
Residential (units)	1,360	129	94	62	31	95	328	739	2,100
Estimated Population	4,000	379	276	182	91	279	965	2,172	6,173
Non-Residential (square feet)	1,600,000	459,000	12,000	19,840	181,500	106,000	36,660	815,000	2,415,000
Estimated Jobs	4,090	938	41	97	312	238	364	1,990	6,080

San Pablo Avenue Potential Buildout Summary, San Pablo Avenue Specific Plan

1.2 Community Vision

1.2.1 Overview

The San Pablo community desires a comprehensive redevelopment of the Mixed Use Center South to create a local and regional mixed-use destination as part of the overall vision for revitalization of the San Pablo Avenue Corridor. In accordance with that vision, the following Redevelopment Concept Plan is provided to illustrate key elements, and to serve as the foundation for regulations contained in Section 2.

Guiding Principles

1. **Walkability.**
All aspects of site development shall support a pedestrian-friendly environment. Aspects of streetscape design, building design, and building frontage, shall contribute to a uniquely walkable destination where community members can park once and experience the entire district on foot.
2. **Flexible Mix of Land Uses.**
The community wishes to encourage a wide range of complementary land uses and to provide flexibility to the investment community, in of support long-term economic health.
3. **A High Level of Craft and Character.**
New buildings, streets, and open spaces shall communicate a high degree of craft and quality. New investment shall contribute to and embody the overall sense of authenticity necessary to distinguish the district both locally and regionally.
4. **A Destination for the Community and the Region.**
Investment should attract residents, workers and visitors on a daily basis. Public spaces shall be designed and provide a unique setting for gatherings, exhibits, performances and passive recreation to draw people throughout the region.

1.2.2 Redevelopment Concept Plan

The following Redevelopment Concept Plan depicts the community's vision for new investment. The plans, massing models, and architectural rendering on the following pages illustrate aspects of the recommended and required network of streets, blocks, and public open spaces, and depict possible future building locations, parking areas, and open spaces in accordance with the community's vision. Drawings do not illustrate all aspects of required development, nor do they illustrate all possible build-out scenarios. The city will consider redevelopment of houses along the west side of Chattleton Lane as a component of new development proposals.

Key Elements

1. **Walkable Street Network:** The site shall be organized as a walkable network of streets, blocks, and public open spaces organized primarily parallel to and perpendicular with San Pablo Avenue.
2. **Central Public Green and Plaza:** At the heart of the envisioned build-out scenario is a significant public open space comprised of "Chattleton Green" and "Chattleton Plaza".
3. **Civic and Cultural Anchors:** New cultural anchors shall support the district's role as a home for arts and entertainment. As a primary gathering space, the public green and plaza shall provide a civic and cultural 'heart' for city's the residents, visitors, and extended community. The northern end of the public plaza shall be reserved for a new civic or cultural building, where it will serve to activate the public plaza.
4. **A Mix of Land Uses:** A range of permitted land uses including residential, commercial, workplace, live-work, civic and cultural uses shall contribute to the site's vitality and support long-term economic health. Pedestrian activity-generating uses should be located along both the 'gateway boulevard' and where they front onto the public green and plaza. Retail uses are further encouraged throughout the ground floor of buildings along San Pablo Avenue.
5. **Transit Support:** The site's location along San Pablo Avenue creates an excellent opportunity to locate new investment within walking distance of transit stops, providing alternative modes of transportation to community members and reducing vehicle miles traveled.
6. **A Mix of Housing Types:** To attract a cross section of the city's residents, a range of new housing types are encouraged that will appeal to single individuals, young couples, empty-nesters, seniors, and families.
7. **Workplace Development:** New office development is encouraged to complement the West County Health Center (WCHC) and nearby medical facilities. Additional workplace development is desired in order to locate new jobs within walking distance of transit facilities and professional services.
8. **Creekside Trail:** Along Wildcat Creek, a new creekside trail enhances access to one of the city's natural features, and provides connection to Salesian High School and nearby neighborhoods.
9. **Park-once Accommodations:** The Redevelopment Masterplan on the following page illustrates a mix of on-street parking, off-street surface parking lots, and shared parking structures to accommodate the district's parking demands while maximizing the efficient use of its valuable land.
10. **Community Amenities:** The Redevelopment Masterplan incorporates a community garden and recreational facilities as well as opportunities for additional amenities including water features and children's play areas.



Illustration 1.2.2 Envisioned Redevelopment Masterplan



Views from above San Pablo Avenue.



View from above City Hall.

1.2 Community Vision



The Vision

City of San Pablo

Section 2: Regulating Code

2.1 Overview

2.2 Block and Frontage Development Regulations

2.3 Land Use Definitions

2.4 Parking

2.5 Landscape and Open Space

2.6 Building Design

2.1 Overview

2.1.1 Contents of the Regulating Code

Section 2.2 Block and Frontage Regulations

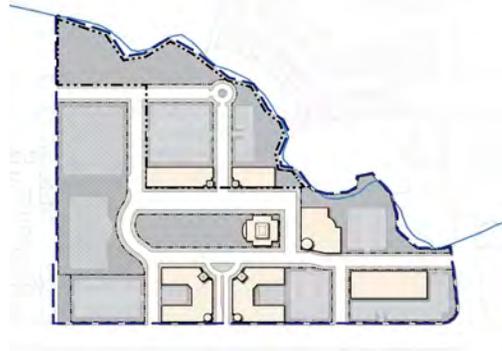
Site development regulations ensure that new investment contributes to the fulfillment of the community's vision, while encouraging innovative design ideas that create an attractive and walkable neighborhood. Regulations are grouped by Block and organized according to Frontage as follows:

Land Use Categories

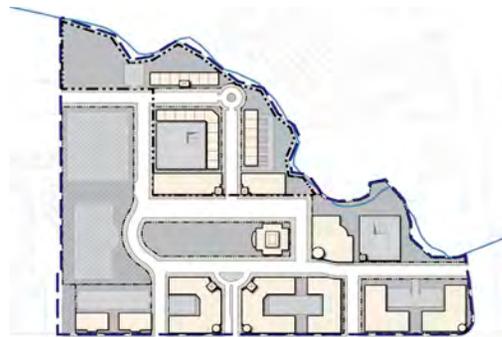
Land use regulations contained herein provide flexibility to developers by permitting a mix of complementary uses for each block. Land use 'categories' are designed to maximize the City's potential for attracting new investment in support of long-term economic health.

Building Placement and Intensity

- **Frontage Coverage:** Frontage coverage is defined as the minimum percentage of the area between the minimum side setbacks that must be occupied by building mass, plazas, or paseos.
 - The purpose of the frontage coverage regulation is to ensure that new development creates attractive and active 'street walls' to enclose the primary public open spaces and support walkability throughout the district.
 - Requirement may be satisfied with one or multiple buildings.
 - A maximum of 10% of building facade may deviate from required frontage line to accommodate entries and building features.
- **Building Setbacks:** Minimum and maximum setback locations relative to the back-of-sidewalk, side, sideyard, rear, alley, or paseo.
- **Building Height:** Minimum and maximum number of floors and total building height. Required setbacks along key frontages.
- **Residential Density:** Maximum dwelling units per acre in accordance with the San Pablo Avenue Specific Plan.
- **Floor Area Ratio:** Total non-residential build-out intensity in accordance with the San Pablo Avenue Specific Plan.



Minimum frontage coverage requirements ensure that new buildings enclose the primary public open spaces. Note: Illustration depicts both required and recommended streets.



Possible build-out scenario illustrating 100% frontage coverage along primary streets. Note: Illustration depicts both required and recommended streets.

Section 2.3 Land Use Category Definitions

Land Use Categories provide flexibility to developers by permitting a mix of complementary uses for each block. For each Land Use Category, a statement of intent and list of exemplary uses is provided.

Section 2.4 Parking Requirements and Strategies

Strategies and standards to encourage applicants to maximize development potential while ensuring an adequate supply of on and off-street parking throughout the district.

Section 2.5 Landscape and Private Open Spaces

Standards and guidelines to ensure that landscapes and private open spaces throughout the district are designed to be safe and attractive while supporting overall walkability.

Section 2.6 Building Design Guidelines

Guiding principles, style guidelines and specific recommendations for commercial / mixed-use buildings, workplace buildings, residential buildings, and parking structures.

2.1.2 Conformance

Standards

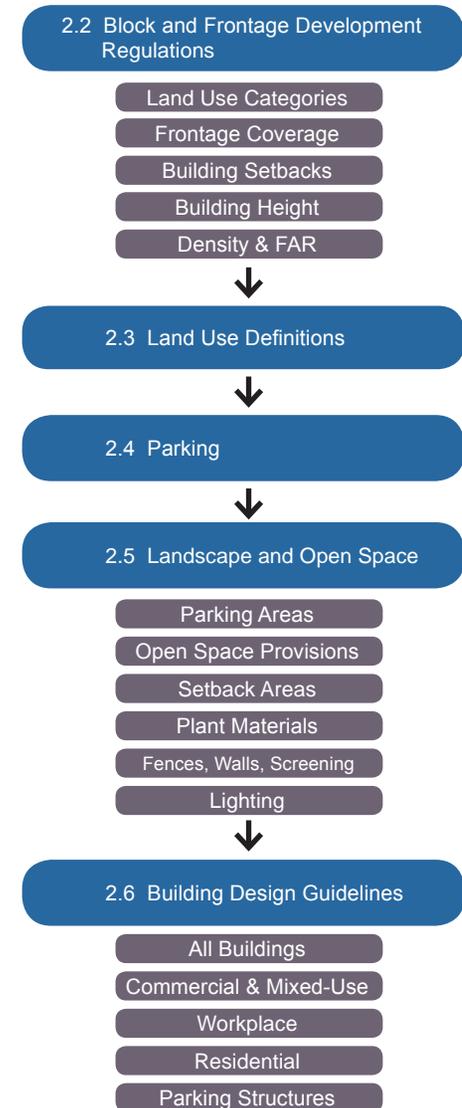
Standards dictate a range of permissible outcomes for each regulation type and may employ values or use words such as “shall” and “shall not”. Conformance with standards is mandatory.

Guidelines

Guidelines are discretionary and suggestive, and contain words and phrases such as “may”, “should”, “should not”, and “are encouraged to” in cases where the community’s preferences may be achieved by a range of outcomes. Conformance with guidelines is not mandatory, however, conformance is encouraged to expedite the approval process.

2.1.3 How to Use the Regulating Code

1. Locate the parcel or block on Illustration 2.2, Map of Blocks and Frontages.
2. Review the corresponding Site Development Standards for Land Use Categories and Building Placement and Intensity; Review Site Design Guidelines.
3. Review section 2.3 for definitions of permitted Land Use Categories
4. Review Section 2.4 for strategies and regulations pertaining to Parking.
5. Review Section 2.5 for standards and guidelines for Landscapes and Private Open Spaces.
6. Review Section 2.6 for design guidelines pertaining to Building Design.
7. Meet with City Staff to review the project prior to submitting a formal application - Staff input can be critical prior to site and architectural design review.
8. Submit appropriate project applications.



2.2 Block and Frontage Regulations

Overview

The block structure generated by the Redevelopment Masterplan aims to maximize developable parcels while emphasizing walkability, pedestrian comfort, access and circulation, and the establishment of a distinguished public realm.

The Blocks and Frontages Diagram on the opposite page illustrates the overall structure of new streets, blocks, and open spaces. Blocks A-E are defined by existing and required new streets illustrated on the diagram. New streets in Block F could be arranged in several ways that support the land use mix and character envisioned in the Redevelopment Masterplan. See Section 3.1.2 for requirements.

Block and parcel dimensions have been tested for basic development feasibility in relationship to anticipated building types.

2.2 Blocks and Corridor Frontages

Blocks

Block A: 36,434 square feet ~ .84 acres

232' frontage on San Pablo Avenue,
157' frontage on City Street

Block B: 27,969 square feet ~ .64 acres

160' frontage on Chattleton Square East,
145' frontage on Gateway Boulevard
160' frontage on San Pablo Avenue
180' frontage on City Street

Block C: 55,368 square feet ~ 1.27 acres

160' frontage on Chattleton Square East,
145' frontage on Gateway Boulevard
314' frontage on San Pablo Avenue
174' frontage on City Street

Block D: 57,643 square feet ~ 1.32 acres

325' frontage on San Pablo Avenue
500' frontage on City Streets
174' frontage on Church Lane

Block E: 67,442 ~ 1.54 acres

180' frontage on Chattleton Square North
480' frontage on City Streets

Block F: 178,892 ~ 4.1 acres

475' frontage on Chattleton Square West
260' frontage on Neighborhood Streets (illustrated)
790' frontage on Neighborhood Streets (anticipated)

Note:

Block dimensions listed above are approximate, and were based on measurements derived from aerial photography.

Corridor Frontages in order of hierarchy

 **Chattleton Square (all sides)**

 **Gateway Boulevard**

 **San Pablo Avenue**

 **Neighborhood Street**

 **Church Lane**

 **City Street**

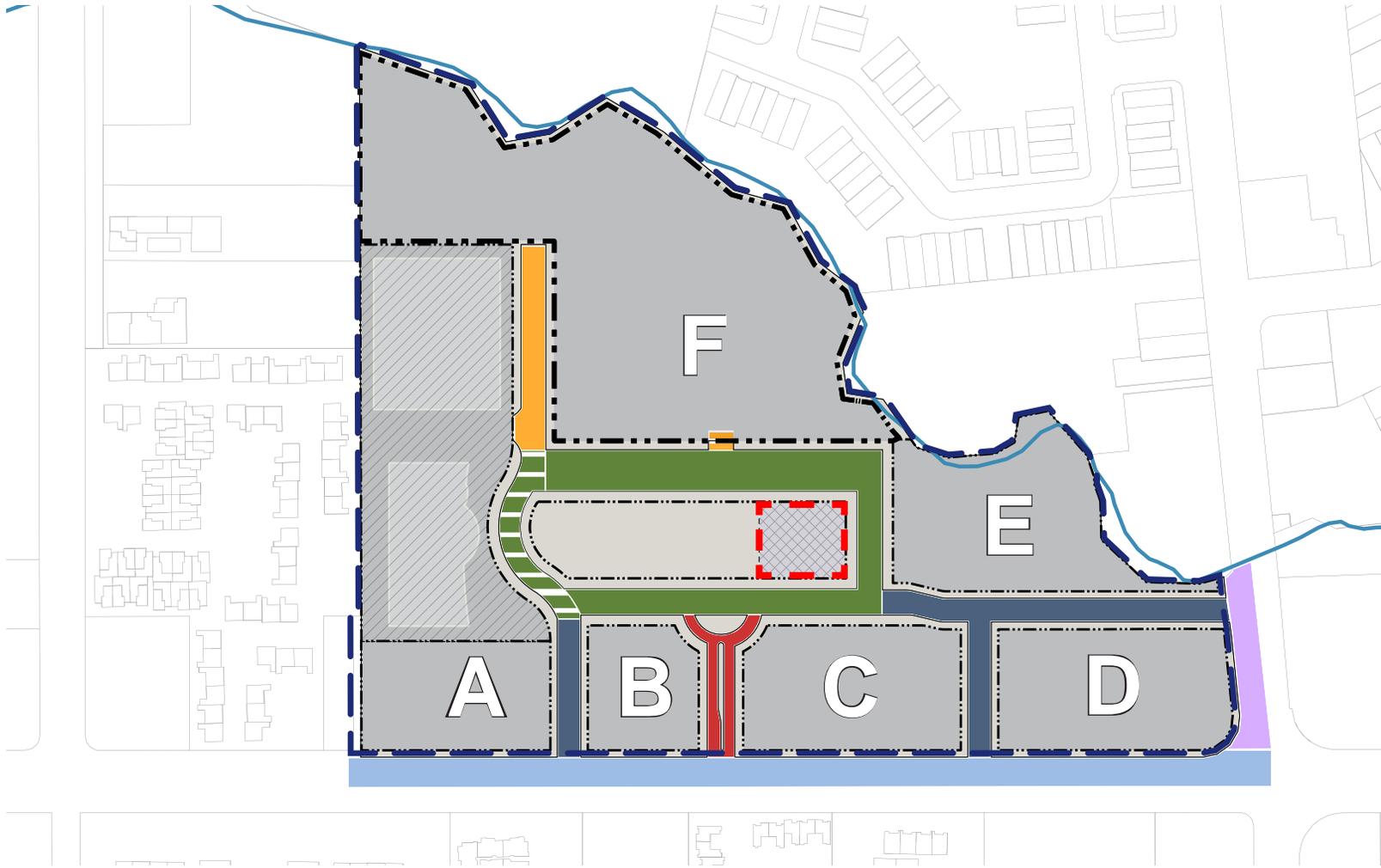


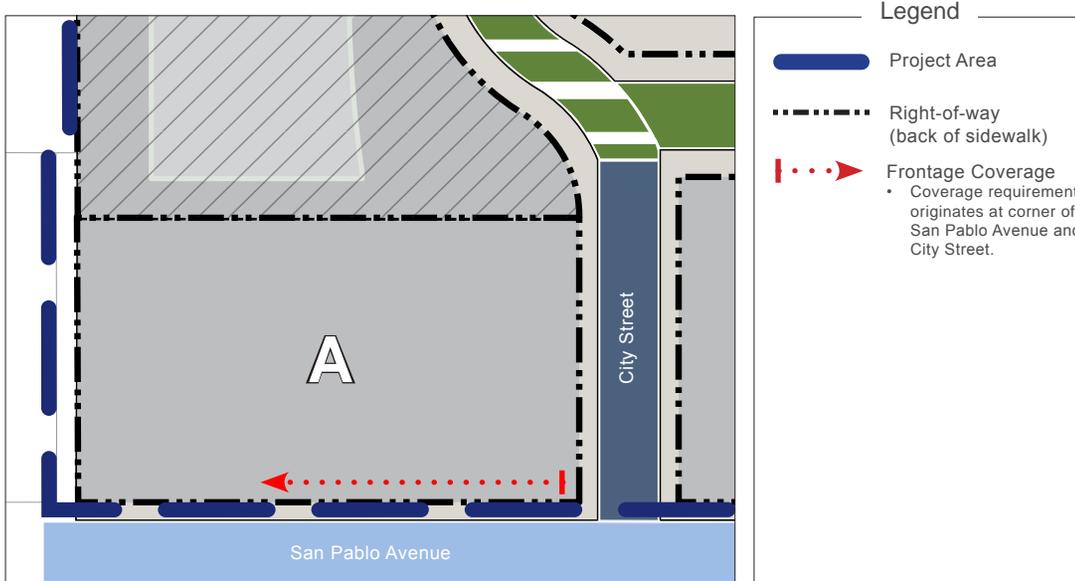
Illustration 2.1
Map of Blocks and Frontages

— — — Reserved for civic/cultural anchor.



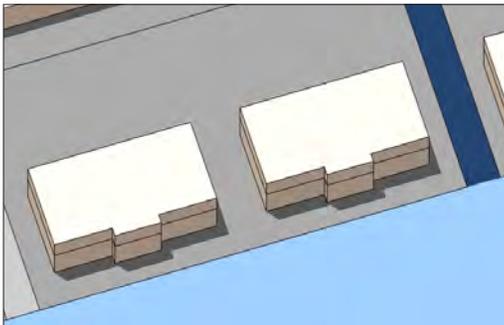
2.2 Block and Frontage Regulations

2.2.A Block A

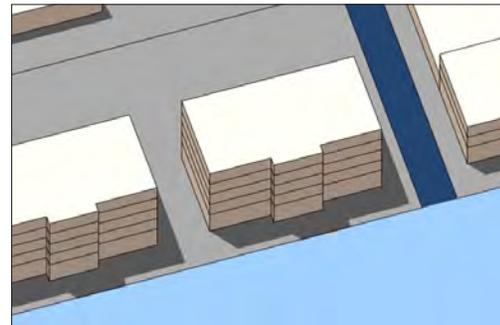


Overview

Block A is approximately .85 acre and includes over 230 feet of frontage along San Pablo Avenue at the project's southern boundary. New investment on Block A will benefit from strong visibility along San Pablo Avenue as well as its proximity to West County Health Center. A new city street connects block A to the central public space, enhancing circulation and access for a range of potential development types. Block A may be an ideal location for shared parking facilities to support development on Blocks A and B.



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height



High-intensity build-out scenario
 - 100% frontage coverage

SITE DEVELOPMENT STANDARDS

1 Land Use Categories

Frontage	San Pablo Avenue	City Street
Main Street Retail	conditional	---
General Commercial	permitted	permitted
Destination Retail	permitted	permitted
Workplace	permitted	permitted
Live/Work	permitted	permitted
Residential	permitted	permitted
Lodging	permitted	permitted
Civic / Cultural	permitted	permitted

g = ground floor u = upper story --- = not permitted

2 Building Placement & Intensity

Frontage	San Pablo Avenue	City Street
Frontage Coverage Requirement		
Minimum	70%	NA
Building Setback		
Front (min-max)	0' - 20'	5' - 20'
Side street (min)	0'	5'
Side yard (min)	0'; 10'@res	5'
Rear/alley (min)	0'; 10'@res	5'
Paseo/court (min)	0'	5'
Space b/w buildings (min)	10'	10'
Corner treatment	NA	NA
Building Height		
Minimum	2 floors, 26'	1 floor, 16'
Maximum	6 floors, 60'	4 floors, 50'
Stepback	NA	NA
Density & FAR		
Residential density (du/ac)	12.1 - 60	12.1 - 60
FAR (max)	2.5	2.5

SITE DEVELOPMENT GUIDELINES

1. Access and Circulation

Pedestrian

1. Primary pedestrian access to ground floor uses should be from San Pablo Avenue.
2. Upper story uses should be accessed directly from San Pablo Avenue through a central lobby, or interior stairwell contained within the main building volume. Secondary access from rear or side of buildings is also appropriate.
3. A pedestrian paseo is recommended to strengthen connection between new development and the West County Health Center plaza.

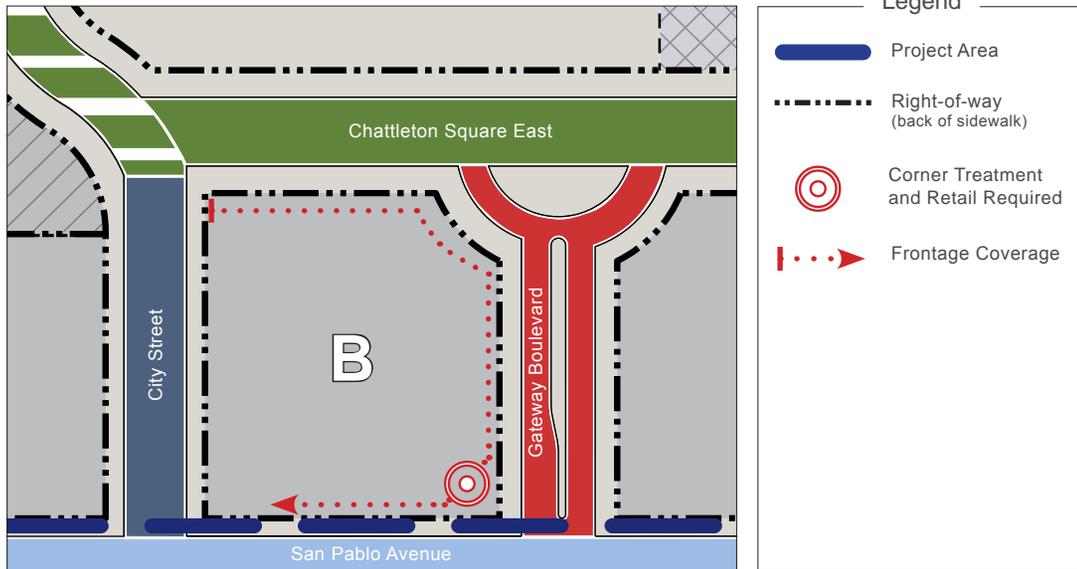
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1. Curb-cuts along San Pablo Avenue should be avoided, and limited to outbound traffic.
2. Parking shall be located towards the rear and/or side of the primary building(s), and should be accessed from City Streets and alleys.
3. On-site residential parking should be in garages or car ports accessed from an alley where possible.
4. On-site workplace and commercial parking should be accessed from the City Street or an alley where possible.



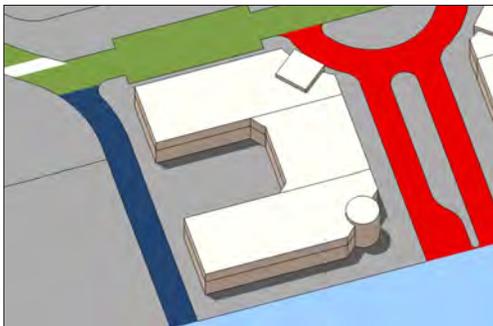
2.2 Block and Frontage Regulations

2.2.B Block B

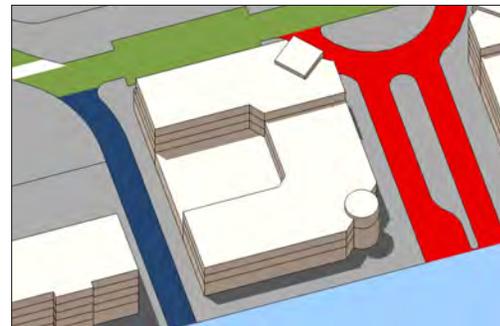


Overview

Approximately .65 acres, Block B flanks the south side of the Gateway Boulevard and extends between San Pablo Avenue, Chattleton Square East, and a City Street to the south. Multistory buildings having ground-floor activity generating retail uses along Gateway Boulevard and Chattleton Square East enhance the pedestrian realm. Upper stories include opportunities for a range of complementary uses located where they are on-display along San Pablo Avenue and provide views of Chattleton Green and Plaza.



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height



High-intensity build-out scenario
 - 100% frontage coverage

SITE DEVELOPMENT STANDARDS

1. Land Use Categories

Frontage	Chattleton Square East	Gateway Boulevard	San Pablo Avenue	City Street
Main Street Retail	permitted	permitted, required at	permitted	conditional
General Commercial	conditional	permitted	permitted	permitted
Destination Retail	permitted	permitted	permitted	permitted
Workplace	permitted - u	permitted - u	permitted	permitted
Live/Work	permitted	permitted - u	permitted	permitted
Residential	permitted - u	permitted - u	permitted	permitted
Lodging	permitted - u	permitted - u	permitted	permitted
Civic / Cultural	permitted - u	permitted - u	permitted	permitted

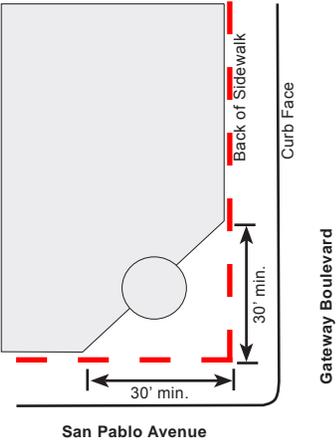
g = ground floor u = upper story --- = not permitted

2. Building Placement & Intensity

Frontage	Chattleton Square East	Gateway Boulevard	San Pablo Avenue	City Street
Frontage Coverage Requirement				
Minimum	100%	100%	70%	NA
Building Setback				
Front (min-max)	0' - 0'	0' - 0'	0' - 20'	5' - 20'
Side street (min)	0'	0'	0'	5'
Side yard (min)	0'	0'	0'; 10'@res	5'
Rear/alley (min)	0'	0'	0'; 10'@res	10'
Paseo/court (min)	0'	0'	0'	5'
Space b/w buildings (min)	0'	0'	10'	5'
Corner treatment	NA	required	NA	NA
Building Height				
Minimum	2 floors, 24'	1 floor, 16'	2 floors, 24'	1 floor, 16'
Maximum	6 floors, 60'	6 floors, 60'	6 floors, 60'	4 floors, 50'
Stepback	5' min above 24'	NA	NA	NA
Density & FAR				
Residential density (du/ac)	12.1 - 60	12.1 - 60	12.1 - 60	12.1 - 60
FAR (max)	2.5	2.5	2.5	2.5

SITE DEVELOPMENT STANDARDS

3. Corner Treatment



Buildings shall include a special setback and significant architectural expression at the corner of San Pablo Avenue and the Gateway Boulevard to enhance visibility and emphasize the site's gateway location.

- 1. The primary building mass shall not encroach within 30 feet of the intersection of the San Pablo Avenue and Gateway Boulevard property lines to create a 'clear zone'
- 2. A special building treatment containing an entry may encroach no more than 10 feet into the clear zone.

SITE DEVELOPMENT GUIDELINES

1. Access and Circulation

Pedestrian

- 1. A prominent building mass should be integrated into the corner treatment at the intersection of San Pablo Avenue and Gateway Boulevard.
- 2. Ground-floor uses should be accessed primarily from the sidewalk via storefronts.
- 3. Upper story uses should be primarily accessed directly from the San Pablo Avenue and/or Gateway Boulevard sidewalk through formal entries leading to a central lobby.

Auto

- 1. Curb-cuts along San Pablo Avenue should be avoided, and limited to outbound traffic.
- 2. Parking facilities and services should be primarily accessed from the City Street or alley where possible.

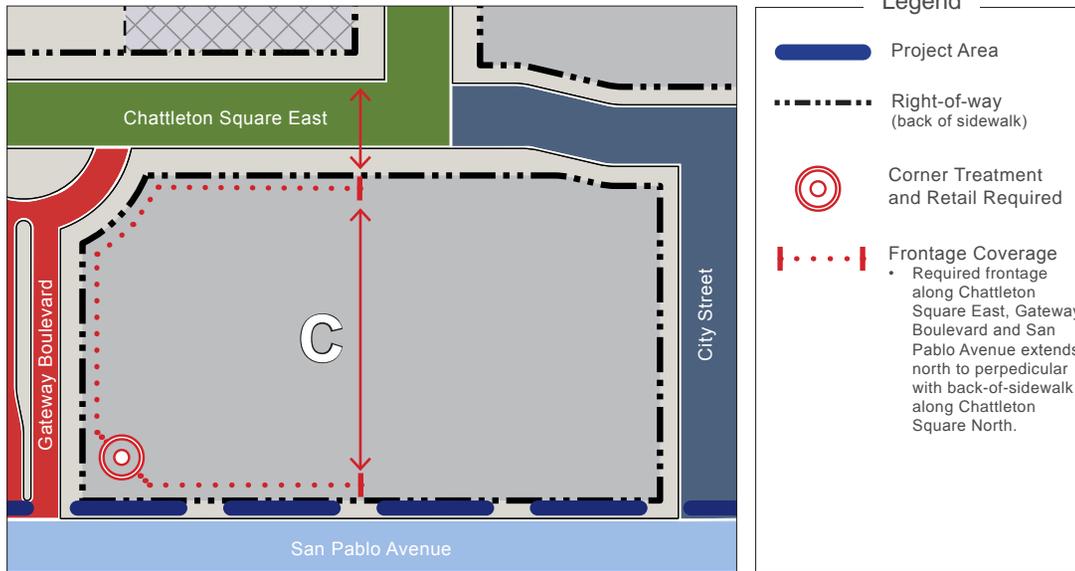
2. Building Design

- 1. Single-story building volumes along Chatleton Square East, Gateway Boulevard, and San Pablo Avenue should be designed as "grand" buildings; interior ceiling height shall be no less than 14 feet, and total building height shall be no less than 24 feet.
- 2. Building design should emphasize the gateway location; Facades along San Pablo Avenue, Gateway Boulevard, and Chatleton Square East shall be constructed to a similarly high degree of craft and character and shall consist of high-quality durable materials. Storefronts should incorporate decorative lighting, awnings, and high quality window treatment to create a high quality attractive environment that supports pedestrian activity.
- 3. Upper story uses should include balconies, terraces, and roof decks to enhance overall vibrancy.



2.2 Block and Frontage Regulations

2.2.C Block C

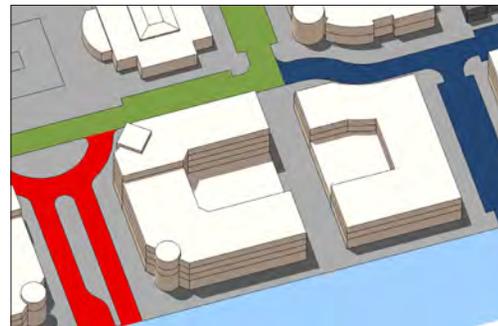


Overview

Slightly larger than 1.25 acres, Block C flanks the north side of the Gateway Boulevard and extends between San Pablo Avenue, Chattleton Square West, and a new City Street to the north. Multistory buildings having ground-floor retail along Gateway Boulevard and Chattleton Square East enhance the pedestrian realm. Upper stories include opportunities for a range of complementary uses located where they are on-display along San Pablo Avenue and provide views of Chattleton Green and Plaza.



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height



High-intensity build-out scenario
 - 100% frontage coverage (incorporates paseo)

SITE DEVELOPMENT STANDARDS

1. Land Use Categories

Frontage	Chattleton Square East	Gateway Boulevard	San Pablo Avenue	City Street
Main Street Retail	permitted	permitted, required at	permitted	conditional
General Commercial	conditional	permitted	permitted	permitted
Destination Retail	permitted - u	permitted	permitted	permitted
Workplace	permitted - u	permitted - u	permitted	permitted
Live/Work	permitted	permitted - u	permitted	permitted
Residential	permitted - u	permitted - u	permitted	permitted
Lodging	permitted - u	permitted - u	permitted	permitted
Civic / Cultural	permitted - u	permitted - u	permitted	permitted

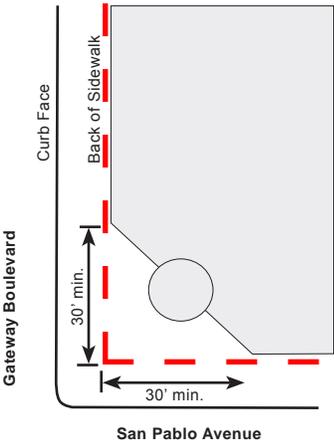
g = ground floor u = upper story --- = not permitted

2. Building Placement & Intensity

Frontage	Chattleton Square East	Gateway Boulevard	San Pablo Avenue	City Street
Frontage Coverage Requirement				
Minimum	100%	100%	70%	NA
Building Setback				
Front (min-max)	0' - 0'	0' - 0'	0' - 20'	10' - 20'
Side street (min)	0'	0'	0'	5'
Side yard (min)	0'	0'	0'; 10'@res	5'
Rear/alley (min)	0'	0'	0'; 10'@res	10'
Paseo/court (min)	0'	0'	0'	5'
Space b/w buildings (min)	0'	0'	10'	10'
Corner treatment	NA	required	NA	NA
Building Height				
Minimum	2 floors, 24'	1 floor, 16'	2 floors, 24'	1 floor, 16'
Maximum	6 floors, 60'	6 floors, 60'	6 floors, 60'	4 floors, 50'
Stepback	5' min above 24'	NA	NA	NA
Density & FAR				
Residential density (du/ac)	12.1 - 60	12.1 - 60	12.1 - 60	12.1 - 60
FAR (max)	2.5	2.5	2.5	2.5

SITE DEVELOPMENT STANDARDS

3. Retail Corner Treatment



Buildings shall include a special setback and significant architectural expression at the corner of San Pablo Avenue and the Gateway Boulevard to enhance visibility and emphasize the site's location as a gateway.

1. The primary building mass shall not encroach within 30 feet of the intersection of the San Pablo Avenue and Gateway Boulevard property lines to create a 'clear zone'
2. A special building treatment containing an entry may encroach no more than 10 feet into the clear zone.

SITE DEVELOPMENT GUIDELINES

1. Access and Circulation

Pedestrian

1. A prominent building mass should be integrated into the corner treatment at the intersection of San Pablo Avenue and Gateway Boulevard.
2. Ground-floor uses should be accessed primarily from the sidewalk via storefronts.
3. Upper story uses should be primarily accessed directly from the San Pablo Avenue and/or Gateway Boulevard sidewalk through formal entries leading to a central lobby.

Auto

1. Curb-cuts along San Pablo Avenue should be avoided, and limited to outbound traffic.
2. Parking facilities and services should be primarily accessed from the City Street or alley where possible.

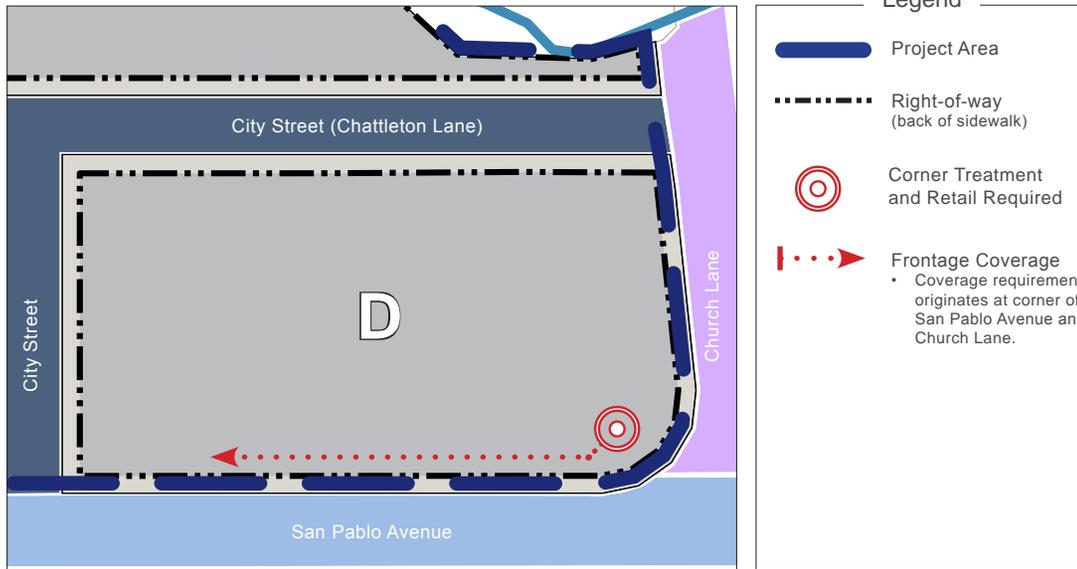


2. Building Design

1. Single-story building volumes along Chatleton Square East, Gateway Boulevard, and San Pablo Avenue should be designed as "grand" buildings; interior ceiling height shall be no less than 14 feet, and total building height shall be no less than 24 feet.
2. Building design should emphasize the gateway location; Facades along San Pablo Avenue, Gateway Boulevard, and Chatleton Square East shall be constructed to a similarly high degree of craft and character and shall consist of high-quality durable materials. Storefronts should incorporate decorative lighting, awnings, and high quality window treatment to create a high quality attractive environment that supports pedestrian activity.
3. Upper story uses should include balconies, terraces, and roof decks to enhance overall vibrancy.

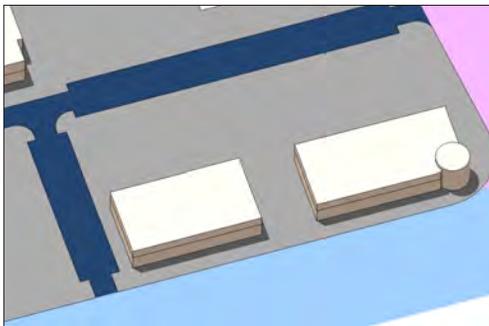
2.2 Block and Frontage Regulations

2.2.D Block D

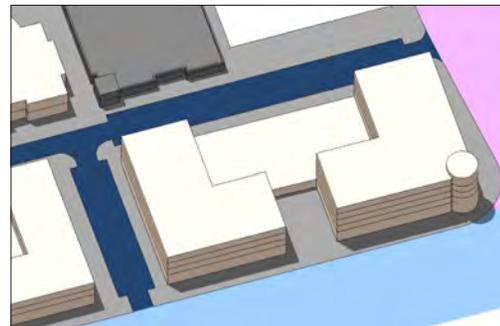


Block D Overview

Encompassing 1.32 acres, Block D forms the northeast corner of the project area. Located at the corner of San Pablo Avenue and Church Lane, Block D is a highly prominent location where new investment will be visible to residents and visitors travelling along one of the region's primary corridors. Multistory buildings having a range of complementary land uses are permitted where they are displayed adjacent to City Hall and easily accessed from Chattleton Lane and a new City Street.



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height



High-intensity build-out scenario
 - 100% frontage coverage

SITE DEVELOPMENT STANDARDS

1. Land Use Categories

Frontage	San Pablo Avenue	City Street	Church Lane
Main Street Retail	conditional	conditional	conditional
General Commercial	permitted	permitted	permitted
Destination Retail	permitted	permitted	permitted
Workplace	permitted	permitted	permitted
Live/Work	permitted	permitted	permitted
Residential	permitted	permitted	permitted
Lodging	permitted	permitted	permitted
Civic / Cultural	permitted	permitted	permitted

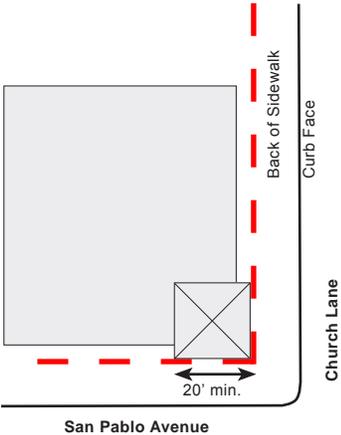
g = ground floor u = upper story --- = not permitted

2. Building Placement & Intensity

Frontage	San Pablo Avenue	City Street	Church Lane
Frontage Coverage Requirement			
Minimum	70%	NA	NA
Building Setback			
Front (min-max)	0' - 20'	5' - 30'	10' - 30'
Side street (min)	0'	5'	NA
Side yard (min)	0'; 10'@res	5'	5'
Rear/alley (min)	0'; 10'@res	10'	10'
Paseo/court (min)	0'	5'	5'
Space b/w buildings (min)	10'	10'	10'
Corner treatment	required	NA	NA
Building Height			
Minimum	2 floors, 24'	1 floor, 16'	1 floor, 16'
Maximum	5 floors, 60'	4 floors, 50'	4 floors, 50'
Stepback	NA	NA	NA
Density & FAR			
Residential density (du/ac)	12.1 - 60	12.1 - 60	12.1 - 60
FAR (max)	2.5	2.5	2.5

SITE DEVELOPMENT STANDARDS

3. Corner Treatment



Buildings shall include a significant architectural expression at the corner of San Pablo Avenue and Church Lane to emphasize the corner’s role as a gateway.

- 1. Buildings should be built to the primary and side street right-of-way line for a distance of at least 20 feet from the corner.

SITE DEVELOPMENT GUIDELINES

1. Access and Circulation

Pedestrian

- 1. A prominent architectural mass such as a corner tower should be used to highlight the intersection of San Pablo Avenue and Church Lane.
- 2. Ground-floor uses should be accessed primarily from the sidewalk via storefronts.
- 3. Upper story uses should be accessed primarily from the sidewalk through formal entries leading to a central lobby, or stairwell contained within the main building volume.
- 4. If more than one building is constructed, a mid-block paseo is encouraged to connect interior parking and/or pedestrian courts to San Pablo Avenue and Chattleton Lane.



Auto

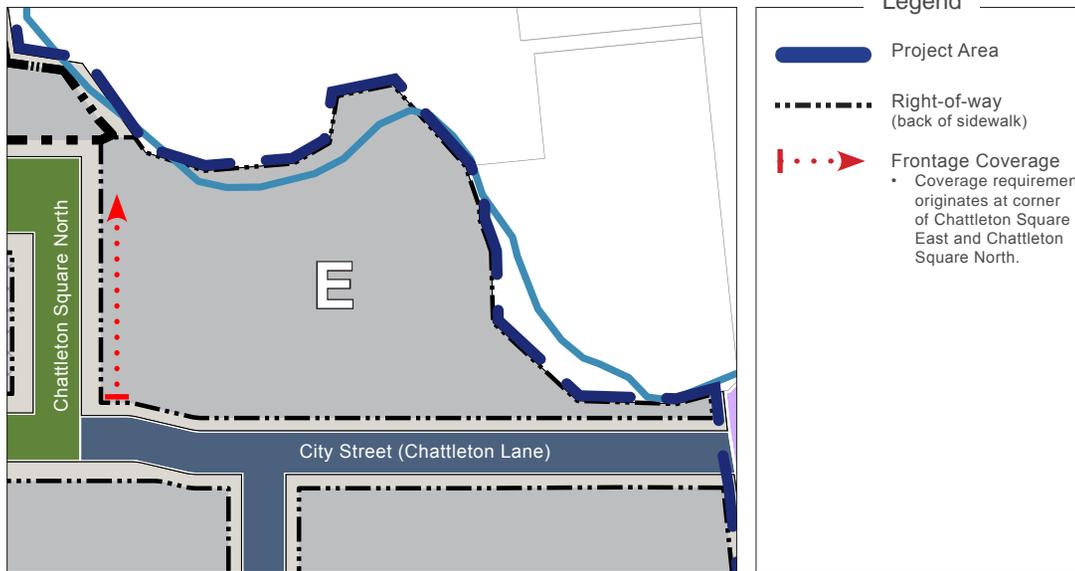
- 1. Curb-cuts along San Pablo Avenue should be avoided, and limited to outbound traffic.
- 2. Parking and services should be primarily accessed from the City Street or alley where possible.

2. Building and Site Design

- 1. Building design should emphasize the corner location and may include a significant architectural expression such as a corner tower containing a formal entry to emphasize the intersection of San Pablo Avenue and Church Lane.
- 2. Storefronts should emphasize large transparent windows, and incorporate attractive awnings and decorative lighting to create a distinguished, high-quality pedestrian environment.
- 3. Alcoves, balconies, terraces, and roof decks are encouraged on upper stories.

2.2 Block and Frontage Regulations

2.2.E Block E



Block E Overview

Encompassing over 1.5 acres, Block E is bound by Chattleton Square North, Chattleton Lane, and Wildcat Creek. With approximately 180 feet of frontage visible from Chattleton Green, Block E is an ideal location for a commercial, civic, or cultural anchor that serves as a prominent visual terminus for pedestrians within the central square. Frontage along Chattleton Lane presents an opportunity for a shared parking lot or structure where it is directly accessible from Chattleton Lane and San Pablo Avenue.



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height



High-intensity build-out scenario
 - 100% frontage coverage

SITE DEVELOPMENT STANDARDS

1. Land Use Categories

Frontage	Chattleton Square North	City Street
Main Street Retail	permitted	---
General Commercial	permitted	permitted
Destination Retail	permitted	permitted
Workplace	permitted	permitted
Live/Work	permitted	permitted
Residential	permitted	permitted
Lodging	permitted	permitted
Civic / Cultural	permitted	permitted

g = ground floor u = upper story --- = not permitted

2. Building Placement & Intensity

Frontage	Chattleton Square North	City Street
Frontage Coverage Requirement		
Minimum	70%	NA
Building Setback		
Front (min-max)	0' - 20'	5' - 20'
Side street (min)	0'	5'
Side yard (min)	0'	5'
Rear/alley (min)	0'	5'
Paseo/court (min)	0'	5'
Space b/w buildings (min)	0'	10'
Creekside Trail	30'	30'
Building Height		
Minimum	1 floors, 24'	1 floor, 16'
Maximum	6 floors, 60'	4 floors, 50'
Stepback	NA	NA
Density & FAR		
Residential density (du/ac)	12.1 - 60	12.1 - 60
FAR (max)	2.5	2.5

SITE DEVELOPMENT GUIDELINES

Access and CirculationPedestrian

1. Ground-floor uses should be accessed primarily from the sidewalks along Chattleton Green North and Chattleton Lane.
2. Upper story uses should be accessed primarily from the sidewalk through formal entries leading to a central lobby. Additional entries may be located on the rear or side of buildings, and accessed from pedestrian paseo or court.
3. Pedestrian connections to the Creekside Trail should be located prominently, and appear as natural extensions of the sidewalk and trail network.

Auto

1. Access to parking facilities and services should be primarily from Chattleton Lane. A curb cut may be located on the northwest side of Chattleton Lane on center with the City Street extending to San Pablo Avenue to enhance auto access to parking facilities.
2. Curb-cuts along Chattleton Square North are strongly discouraged. A curbcut may be located at the intersection of Chattleton Square North and Chattleton Square West to provide access to parking facilities located behind the primary buildings.

**Building and Site Design**

1. Building design should emphasize the corner location and may include a significant architectural expression or facade treatment to link Chattleton Square with Chattleton Lane, a primary connector to City Hall.
2. The building's southeast facade (fronting on Chattleton Square North) shall utilize materials and design features to create a highly attractive visual terminus for the square.
3. Balconies and terraces are encouraged on upper floors to add vibrancy and activate the square. A roof deck is particularly encouraged where it will provide views onto Chattleton Square and Plaza.

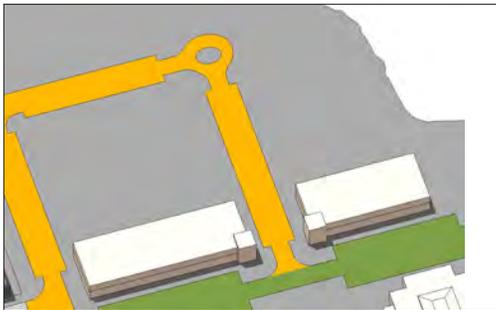
2.2 Block and Frontage Regulations

2.2.F Block F



Block F Overview

Encompassing approximately acres 4 acres, Block F extends between the western edge of Chattleton Square and Wildcat Creek. Multistory buildings fronting along Chattleton Square West create opportunities for a range of complementary land uses where they have excellent views of the public open spaces. Residential and workplace uses are permitted in buildings located along new Neighborhood Streets, recommended as part of new development. Applicants are required to provide a Neighborhood Street connection to the southern project boundary in anticipation of a future connection to Vale Road. (See Section 3.1.2)



Low intensity build-out scenario
 - Minimum required frontage coverage
 - Minimum required building height
 Note: Street network is flexible (See section 3.1.2)



High-intensity build-out scenario
 - 100% frontage coverage
 Note: Street network is flexible (See section 3.1.2)

SITE DEVELOPMENT STANDARDS

1. Land Use Categories

Frontage	Chattleton Square West	Neighborhood Street
Main Street Retail	permitted, required at	---
General Commercial	permitted	---
Destination Retail	permitted	---
Workplace	permitted	permitted
Live/Work	permitted	permitted
Residential	permitted - u	permitted
Lodging	permitted	permitted
Civic / Cultural	permitted	permitted

g = ground floor u = upper story --- = not permitted

2. Building Placement & Intensity

Frontage	Chattleton Square West	Neighborhood Street
Frontage Coverage Requirement		
Minimum	100%	NA
Building Setback		
Front (min-max)	0' - 0'	10' - 20'
Side street (min)	0'	5'
Side yard (min)	0'	5'; 10' if diff use
Rear/alley (min)	5'	10'
Paseo/court (min)	0'	5'
Space b/w buildings (min)	0'	10'
Creekside Trail	30'	30'
Building Height		
Minimum	2 floors, 24'	1 floor, 16'
Maximum	5 floors, 60'	4 floors, 50'
Stepback	5' min above 24'	NA
Density & FAR		
Residential density	12.1 - 60 du/ac	12.1 - 60 du/ac
FAR (max)	2.5	2.5

SITE DEVELOPMENT GUIDELINES

Access and CirculationPedestrian

1. For buildings fronting Chattleton Square West, ground-floor retail, commercial or workplace uses should be accessed primarily from adjacent sidewalks via storefronts. Upper story uses should be accessed primarily from the sidewalk through formal entries leading to a central lobby, or stairwell contained within the main building volume.
2. For buildings fronting onto Neighborhood Streets, ground floor workplace uses should be accessed primarily from the adjacent sidewalk via formal entries. Ground-floor residential uses may be accessed via stoops or formal entries leading to a central lobby or interior stairwell.
3. New development should incorporate pedestrian connections to the Creekside Trail.
4. A mid-block paseo is encouraged to connect the Chattleton Square West sidewalks to interior parking facilities, plazas, or courts.

Auto

1. Curb-cuts along Chattleton Square West are strongly discouraged. Parking facilities and services should be accessed from the City Streets and Neighborhood Streets.
2. On-site residential parking should be in podiums, garages or car ports accessed from City and Neighborhood Streets where possible.
3. On-site workplace and commercial parking should be in garages, structures or surface lots accessed from City and Neighborhood Streets where possible.

Building and Site Design

1. Building design should celebrate Block F's frontage along Chattleton Square West. Storefronts should emphasize large transparent windows, and incorporate attractive awnings and decorative lighting to create a distinguished, high-quality pedestrian environment.
2. Building facades along Chattleton Square West should be designed with a high degree of craft and character to enhance the overall attractiveness of the public realm. Retail is required where buildings front directly onto Chattleton Plaza. If a Neighborhood Street is present, retail should be accessed through entries located at the building corner.
3. Terraces, porches, alcoves, balconies, and roof decks are encouraged to add visual interest and create outdoor rooms.
4. Buildings along Neighborhood Streets may front onto the primary street, interior courts and paseos. Buildings shall not front onto parking areas.
5. Along Neighborhood Streets, new development should be designed to be compatible with both residential and workplace development: Building massing should be designed to articulate the extents of individual dwellings or structural bays. Offsetting of bays and use of corresponding roof forms is encouraged.
6. Building volumes along Neighborhood Streets should front onto the primary street and may be arranged to create outdoor spaces such as courts, paseos, and yards.
7. Development adjacent to Wildcat Creek shall be designed in coordination with the Creekside Trail and Pedestrian Bridge identified in Section 3.2.3.



2.3 Land Use

Objectives

“Develop the Circle-S site with a vital mix of synergistic retail, eating/ drinking establishments, office, and residential uses, and as a citywide and regional destination.”

San Pablo Avenue Specific Plan

2.3.1 Overview

Land uses in the Mixed Use Center South site shall support the community’s vision for a walkable, pedestrian-friendly district that attracts residents and visitors as part of their daily lives. All uses shall be compatible with, and cater to the needs of the community and the region. This code organizes land uses into the following eight Use Categories: *Main Street Retail, General Commercial, Destination Retail, Workplace, Live/Work, Residential, Lodging, Civic & Cultural*. When the content of this section conflicts with the San Pablo Municipal Code, this Code shall govern.

For each Land Use Category, a statement of intent and list of exemplary uses is provided. The lists do not identify all of the potentially appropriate uses. Each application will be evaluated in terms of its conformance with the intent of the *Land Use Category* and the “Overview” statement for the respective block location. Determination of “similar” uses is subject to review by the Planning Director and may be referred to the Planning Commission.

Conditional Uses

Uses listed as “Conditional” shall be reviewed by the Planning Commission for their appropriateness in support of the community’s vision.

2.3.2 Categories

Main Street Retail

Intent: Main Street uses offer goods and services that attract and sustain pedestrian activity in support of a vibrant, walkable, and diverse environment for shopping, strolling, dining and entertainment.

Permitted Uses

- Eating establishments including and similar to: bistros, cafes, delicatessens, restaurants. Outdoor dining is encouraged.
- Personal and Business Services that generate significant foot traffic including and similar to: hair and nail salons, clothing alterations, copy shops, dance studios, fitness and health clubs, internet cafe’s, martial arts, massage and physical therapy, shoe and luggage repair, yoga studios etc.
- Performing arts theaters and art galleries.
- Specialty food sales including and similar to: bakeries, cheese shops, delicatessens, ice cream parlors, gourmet and health foods, wine shops etc.
- Food markets
- Retail sales including and similar to: antiques, apparel, accessories, art supplies, bicycles, books, flowers, boutique furniture, housewares, music and musical instruments, pharmacy, photographic equipment, sporting goods, toys, video rental etc.

Conditional Uses

- Uses operating between 10:00 pm and 7:00 am.
- Bars, nightclubs, and similar uses serving alcoholic beverages not clearly ancillary to food sales.

Prohibited Uses

- Uses that detract from the pedestrian-oriented character and synergy.

General Commercial

Intent: General Commercial uses include sales and services that do not necessarily create significant pedestrian activity and generally serve customers arriving by automobile. Uses should not directly compete with Main Street Retail Uses for tenants and / or customers.

Permitted Uses

- Business services including and similar to banks and financial institutions, catering services, print services, real-estate agencies, travel agencies etc.
- Eating establishments over 3,000 square feet.
- Educational services including and similar to: colleges, adult schools, music schools etc.
- Entertainment uses including and similar to: live entertainment, amusement centers, indoor entertainment.
- Food stores over 5,000 sq. ft. including and similar to grocery stores, markets, and packaged goods.
- Personal services including and similar to: adult care, child care, dance and gymnastics training centers, health spas, exercise clubs, martial arts.
- Retail sales including and similar to: apparel, auto sales, computer supply, decorator arts and design center, home furnishings, household appliances and electronics, lamps/lighting, office supply, pharmacies, sporting goods, etc.

Conditional Uses

- Auto-parts sales. *All merchandise must be displayed within the primary building.*
- Eating and drinking establishments under 3,000 square feet.
- Bars, nightclubs, and similar uses serving alcoholic beverages not clearly ancillary to food sales.

Destination Retail

Intent: Destination Retail uses create regional draw, helping to attract visitors and tourists to San Pablo.

Permitted Uses

- Large-scale indoor entertainment venues including and similar to sports complexes, movie theaters, music, and entertainment venues.

Workplace

Intent: Workplace uses include professional office and business services. Where conditionally permitted, uses shall be compatible with residential uses, and support the district's pedestrian-friendly environment.

Permitted Uses

- Administrative, business and professional uses including and similar to accounting and financial, telecommunications, design related, real estate, research and development etc.
- Educational and Instructional Facilities.
- Professional offices & services including and similar to medical and dental offices.

Live/Work

Intent: Live/work uses combine residences with compatible work-related activities to support pedestrian activity and appeal to a wide-range of community members.

- Live-work: An integrated dwelling and workplace. Work activity shall be registered as a business and may include residentially-compatible practices such as arts, crafts, graphic design, photography, jewelry-making, and similar activities, personal services and business services including and similar to art-therapy, financial planning, physical therapy, marriage and family therapy etc. Activities that require hazardous material, or generate significant noise are not permitted.

Residential

Intent: The Residential Use Category includes a range of permitted residential development types that support the community's vision for safe, active, and pedestrian-oriented neighborhoods. Residential development within the Plan Area shall have a minimum of 12.1 and a maximum of 60 units per gross acre in accordance with the Medium and High Density Residential designation in the San Pablo Avenue Specific Plan.

Permitted Types

- Multiple Family Dwellings including apartments, flats, town-houses and/or lofts, located in a multistory residential or mixed-use building.
- Single-Family Attached dwellings including row-houses and duplexes.
- Home occupations upon compliance with the standards as set forth in the San Pablo Municipal Code.
- Family Day Care as licensed by the State of California.

Conditional Uses

- Private and religious schools, day care centers.
- Community care facilities not listed under uses permitted.

Lodging

Intent: The Lodging Land Use Category defines lodging types permissible in the Plan Area.

Permitted Uses

- Lodging uses including and similar hotels and motels, inns and bed and breakfast establishments.

Civic & Cultural

Intent: Civic and Cultural uses permit a range of public and quasi-public uses intended to attract a variety of residents and visitors.

Permitted Uses

- Civic and Cultural uses including and similar to government center or office, library, police substation, museum etc.
- Quasi-public uses including and similar to conference centers, teen and senior centers etc.
- Non-profit services, fraternal organizations, clubs, union halls, and similar organizations.
- Public parks and open spaces.
- Public parking facilities.

2.4 Parking

Objectives

Strategies and Requirements seek to provide flexibility for new development by providing on-street parking and encouraging shared parking facilities throughout the district.

2.4.1 Overview

Parking regulations support walkability and use of alternative modes of transportation while recognizing the importance of access to parking supply necessary to support desired new investment. The Plan's overall area in combination with the required network of streets and blocks supports a 'park once' system wherein the majority of residents, visitors, and employees are able to park their vehicles and experience the project area on-foot.



The entire Plan Area falls within a 5 minute walking radius.

Investors that choose to develop new projects do so to be part of one of the City's most walkable destinations. To this end, parking strategies and requirements are established in support of an environment that is friendly to pedestrians, while allowing investors to maximize use of the site's most valuable land for new urban development.

2.4.2 Strategies

A. Create a Comprehensive (District-Wide) Strategy

The City will partner with the investment community to coordinate district-wide parking supply with anticipated parking demand, so as to maximize efficiency and reduce oversupply of surface parking lots.

B. Utilize On-street Parking Supply

The on-street parking capacity for required streets in the Plan Area per the Redevelopment Masterplan is approximately 150-170 parking stalls. The San Pablo Avenue Specific Plan designates on-street parallel parking along the west side of San Pablo Avenue adjacent to the site, which may ultimately provide an additional 35-45 parking spaces, creating a total of approximately 200 on-street public parking stalls.

C. Require Shared Parking

Non-residential parking spaces are encouraged to be shared by more than one user to maximize their utilization. Anticipated land use mix includes residential, retail, workplace, entertainment, lodging, civic and cultural uses, which may create opportunities for significant shared parking arrangements by virtue of their respective peak parking demands. As new development is proposed, an analysis of peak parking demands should be conducted to determine the potential for shared parking arrangements and facilities. The City will work with the West County Health Center to explore opportunities for utilizing available parking spaces within the West County Health Center parking structure for public use and/or as dedicated employee spaces for nearby businesses.

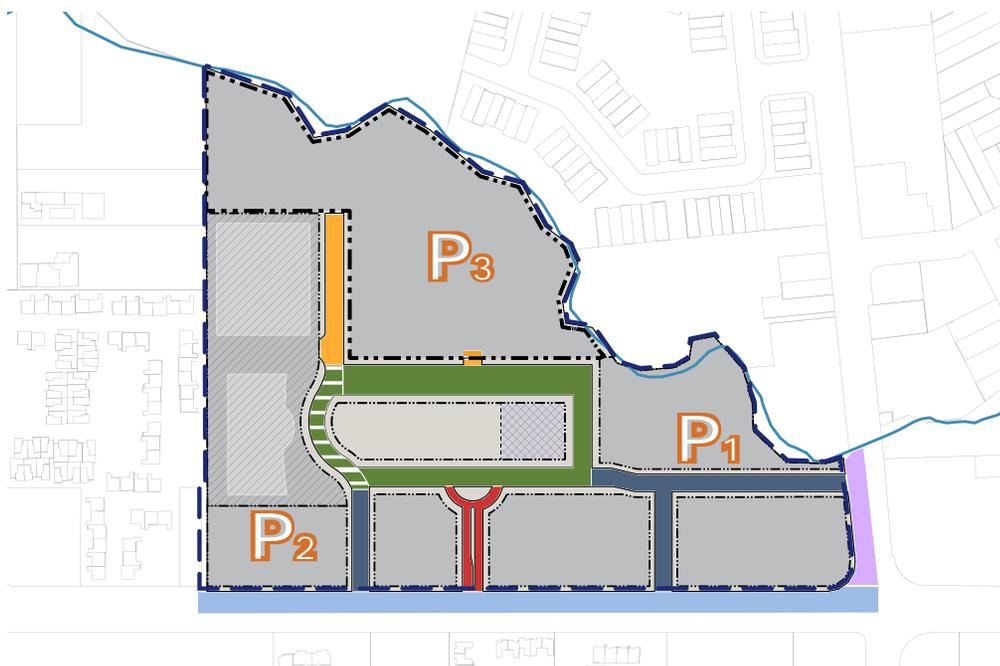
D. Provide Publicly-Accessible Parking Facilities

The City may choose to construct a shared surface lot or parking structure to provide incentive for new investment by accomplishing the following objectives:

- Maximize site build-out potential; enable maximum density (i.e. rentable/saleable/leasable square footage)
- Enhance walkability and pedestrian safety by reducing driveways and curb-cuts.
- Provide parking supply to serve public open spaces, civic, cultural, and special events.

E. Parking Facility Strategies

To support new investment along Chattleton Square and the Gateway Boulevard, publicly accessible parking lots may be developed to serve a range of land uses. As new investment continues to occur, these properties may be transformed from surface lots to structured parking to serve the increasing demand for parking stalls. Preferred locations for publicly accessible parking facilities are illustrated below.



- P1. Along Chattleton Lane.
- P2. Along the southernmost City Street in the Plan Area with additional egress/access from San Pablo Avenue and the service street constructed along the southern property boundary. Parking facilities shall comply with frontage coverage requirements for new buildings.
- P3. Behind (west of) buildings fronting Chattleton Square West.

2.4.3 Standards

1. All new non-residential development (including mixed-use development with a residential component) shall share their non-residential off-street parking spaces when possible to do so.
2. Surface parking lots must meet planting and buffering requirements as detailed in Section 2.4.1 - Landscape and Open Space.
3. Parking requirements may be satisfied through any combination of off-street spaces in shared public or private facilities or on-street public spaces within 200 feet of an entrance.
4. Required Stalls

Land Use	spaces
Main Street Retail (min / max per 1000 sq.ft.)	0 / 4
General Commercial (min / max per 1000 sq.ft.)	0 / 4
Destination Retail (min / max per 1000 sq.ft.)	0 / 6
Workplace (min / max per 1000 sq.ft.)	2 / 6
Live/Work (min / max per 1000 sq.ft.)	0 / 2
Residential	
Studio / 1 bedroom (min / max per unit)	0 / 1.5
2 plus bedrooms (min / max per unit)	1 / 2
Lodging (per lodging unit)	1 / 1.5
Civic / Cultural (min / max per 1000 sq.ft.)	0 / 6

- Minimum and maximum parking requirements apply only to off-street surface lots.
- There is no maximum for structured or underground parking supply
- Residential developments are permitted to add 1 guest space per 4 units in addition to maximum supply.
- Lodging uses are permitted additional spaces dedicated to operators / employees.

5. Dimensions: Standard stall size for surface lots shall be 9 feet wide by 18 feet long. Compact diagonal and head-in parking spaces shall be in accordance with the San Pablo Municipal Code.
6. Access: Parking facilities shall be accessed from Alleys, City Streets, or Neighborhood Streets where possible. Curb cuts along Chattleton Square Streets and the Gateway Boulevard are prohibited. Curb cuts along San Pablo Avenue should be limited.
7. Bicycle Parking: A minimum of 1 bicycle parking mechanism shall be provided for each 10 auto parking spaces in off-street facilities.

2.5 Landscape and Open Space

2.5.1 Overview

This section contains standards and guidelines to ensure that landscapes and open spaces are designed to be attractive and supportive of the community's desire for a high-quality walkable destination.

Contents

1. Parking Areas
2. Setback Areas
3. Private Open Space
4. Plant Materials
5. Fences, Walls, and Screening
6. Lighting

Objectives

Policies and improvements seek to balance the provision of adequate parking with innovative strategies to provide flexibility for mixed-use development and reduced parking requirements to promote transit
San Pablo Avenue Specific Plan

2.5.2 Parking Areas

Standards

1. Parking lots shall be setback 4 feet minimum from public rights-of-way, adjacent property lines, and the Creekside Trail.
2. Parking lots shall include clearly defined pedestrian pathways no less than 4 feet wide along at least one edge of the parking field. Permeable paving materials shall be used where possible to reduce stormwater run-off. Recommended paving materials include open-joint concrete unit pavers, porous asphalt, turf-grids, and similar paving materials specifically designed for automobile parking.
3. Paving materials shall be light in color to reduce overall heat glare.
4. Stormwater shall be collected on-site using flow-through stormwater infiltration planters (flush or with curb inlets).
5. Planters shall be designed to maximize detention, infiltration and on-site treatment of stormwater from adjacent paved areas. Sub-drains may be used to store, filter, and convey water to infiltration planters or similar on-site filtration areas.
6. A minimum of one tree for every six parking spaces shall be planted in flush mounted grates or curbed planting areas between parking stalls. Permanent irrigation shall be provided to all trees.
7. Setback areas (between parking field and right-of-ways) shall be planted with fast-growing deciduous trees at 20 feet on-center maximum spacing. Trees may be located in planting strips or in infiltration planters. Low lying ground cover is also appropriate but shall not obstruct visibility to and from the parking areas.
8. Parking lots shall be illuminated using pedestrian scale decorative lights along pathways and where pedestrians cross vehicular travel ways.

Guidelines

1. Setback areas adjacent to public sidewalks along San Pablo Avenue may include a low decorative wall designed in accordance with building architecture.
2. Decorative ornamental planting and ground covers may be located in infiltration planters to assist with water filtration, uptake and detention.
3. Additional pathways are encouraged to connect parking facilities with the Creekside Trail where applicable.



Permeable concrete pavers in alleys are recommended to decrease stormwater run-off.



Water infiltration and detention systems.

2.5.3 Setback Areas

Standards

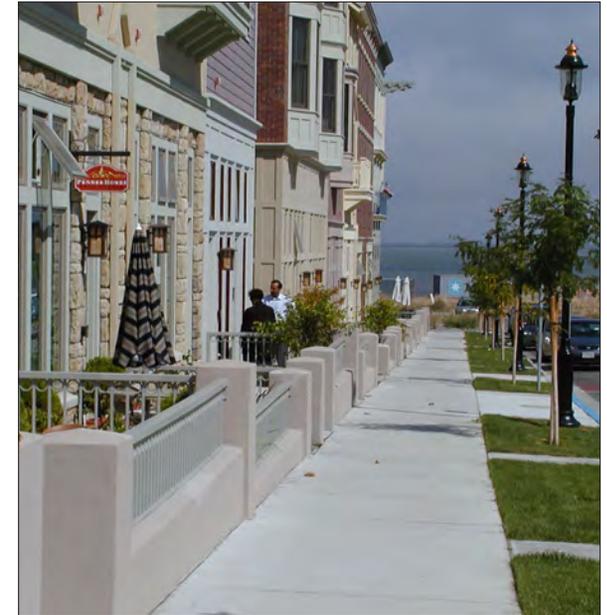
1. No parking space may be located in any required front setback area.
2. Where storefronts are present, front setback areas shall be primarily hardscaped and designed for pedestrian use including outdoor dining, the display of goods, or similar uses.
3. Along Neighborhood Streets and City Streets, front, side street, and sideyard setback areas, and the space between buildings shall be composed of hardscaping, gardens, courts, and other landscape designs that are welcoming and supportive of pedestrian activity.
4. Along San Pablo Avenue frontage, side yard and side street setbacks, and the space between buildings may contain paseos leading to building entrances and parking areas. Hardscaped courts shall utilize decorative paving materials such as concrete pavers, bricks or colored concrete in support of a pedestrian-friendly environment.
5. Along Neighborhood Streets and Chattleton Lane, side yard setback areas adjacent to residential buildings should enhance privacy.
6. Where new development is adjacent to existing single family residences, setback areas shall include sufficient landscape buffering to protect and enhance existing residences.

Guidelines

1. Setback areas along Neighborhood Streets may contain a terraced edge or low decorative wall to physically separate the public sidewalk from private open space.



Terraced setback area at base of modern rowhouse



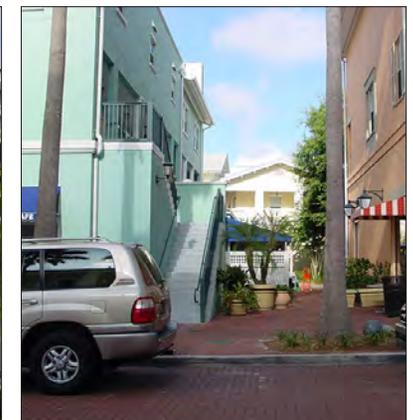
Decorative wall and hardscape at base of live-work building.



Setback paved as extension of sidewalk.



Landscaped front setback area adjacent to multi-family residential development.



Space between buildings paved as extension of sidewalk environment.

2.5 Landscape and Open Space

2.5.3 Private Open Space

Standards

- Provisions: Private open space must be met on-site. Setback areas do not count toward open space provisions.

Development Type	minimum	description
Single-family attached (rowhouses)	200 sq.ft	private yard
Multi-family: upper floor (apt, loft)	40 sq.ft.	balcony, roof terrace
Multi-family: ground floor unit (apt, loft)	40 sq.ft.	yard, court, terrace
Live-work ground floor units	100 sq.ft / unit	private or shared court, yard, roof terrace

- Private yards shall have a minimum dimension of 10 feet.
- Terraces and balconies shall be functional spaces with a minimum dimension of 6 feet.
- At least one tree shall be planted in every private yard and within planters for each 1000 sq.ft. of court and roof deck.

Guidelines

- Courtyards, roof decks and podium decks, and common yards serving multiple dwellings should contain both hardscape and landscape areas and should provide outdoors seating, decorative lighting, and should incorporate shade components.
- Private yards adjacent to townhouses and ground floor dwelling units should be rectangular and should be accessed directly from the corresponding dwelling where possible.



Common court and private terrace.



Shared terrace.



Private roof terraces.



Rowhouses with private roof terraces.



Shared court and private terraces.



Shared court above parking podium.



Private and shared balconies.



Shared court above parking podium.

2.5.4 Plant Materials

Standards

1. All plants shall be drought tolerant and low maintenance.
2. Plant selections shall be made from the palette of the region in accordance with the Section 17.40.030 of the SPMC.
3. A minimum of one tree per 1,000 square feet of private open space is required.
4. All plant materials adjacent to public rights-of-way shall support pedestrian friendly sidewalk conditions. Trees with excessive fruit droppings or shallow roots shall be avoided adjacent to sidewalks.

Guidelines

1. Root zones should contain high-quality soils and should be expanded beneath sidewalks and other impervious surfaces to enhance tree health.
2. Seasonal and year-round flowering shrubs and trees should be used where they can be most appreciated - adjacent to sidewalks and paseos - and where they frame unique building features.
3. Landscape treatment should be used for screening purposes, especially along rear property lines adjacent to the Creekside Trail.
4. See Bay Friendly Landscaping Guidelines for additional direction.



Decorative planting of shared open space includes trees, shrubs and ground covers.

2.5.5 Fences, Walls, and Screening

Standards

1. All fences and walls shall be built with attractive materials in accordance with building architectural style and Section 17.30.060 of the SPMC
2. The solid portion of a fence or wall shall not exceed 3 feet; 'open' design areas such as lattice work, wrought iron, or grille work may extend the total fence height to 6 feet.
3. Where workplace uses are adjacent to residential uses, service bays shall be screened from residential property using landscaping and/or a decorative fence.

Guidelines

1. Fence and wall designs should include a base and cap treatment or may include a series of piers, slats, or ornate grille work.
2. Where visible from public areas, low walls should incorporate decorative elements.
3. Use of security fences should be minimized, and limited to locations where additional safety is needed such as adjacent to the Creekside Trail.
4. Security fences, where needed, should incorporate decorative grille work or artistic elements to enhance their overall appeal.



Masonry piers and iron grillework.

2.5.6 Lighting

Standards

1. Exterior lighting shall include cut-off and other 'dark-sky' technologies to direct light downward except where safety purposes require otherwise.
2. Pedestrian areas including sidewalks, pathways, paseos, parking areas and courts shall be illuminated to increase safety and overall appeal.
3. Area lights shall not throw light onto adjacent residential buildings.
4. Light source height shall not exceed 12 feet for landscape, open space, and pedestrian pathway.
5. Low pressure sodium vapor or other lights casting an orange glow are prohibited.
6. Exterior lighting shall not exceed 0.5 foot candles.

Guidelines

1. Decorative lighting of planted areas and pedestrian pathways is strongly encouraged.
2. Bollard-mounted lights and in-ground lights are encouraged to accent pedestrian pathways.
3. Up-lighting of trees and landscape features is encouraged to enhance overall aesthetic.
4. Light standards should include prismatic lenses, diffusers, or refractors to create soft and efficient distribution, and to avoid 'hot spots' and glare.
5. Photovoltaic or solar collectors are encouraged.



Pedestrian-scale light with 'dark-sky' shielding.

2.6 Building Design

2.6.1 Overview

This section is provided to guide the character of new buildings in accordance with the community's vision for an attractive and walkable town center. New buildings will become a powerful asset to attract and retain desired tenants, and to draw residents and visitors as part of their daily lives. Each new development is responsible for contributing a high-level of craft and design quality in order distinguish the Project Area as a regional destination, and to support long-term economic health.

Contents

1. Guidelines for All Building Types
 - A. General Principles
 - B. Style
 - C. Green Building / Energy Efficiency
2. Guidelines for Specific Building Types
 - A. Commercial and Mixed-Use Buildings
 - B. Workplace Buildings
 - C. Residential Buildings
 - D. Parking Structure

2.6.2 Guidelines for All Building Types



Commercial Mixed-Use Building



Workplace Buildings



Residential Buildings



Parking Structures

A. General Principles

1. Architecture massing and facade articulation work in harmony with structural systems; wall openings along the primary facades shall correspond to internal spaces as feasible.
2. All building facades shall have a high level of finish and articulation. Building facades that are visible from the public realm (including streets and paseos) shall be designed and finished to be consistent with the primary building facade.
3. Building elements shall be real and operational. Pastiche shall be avoided.
4. The design of ancillary buildings including the style of roofs, windows, doors, building materials and colors, and overall architectural style shall be consistent with those of the primary building.
5. Buildings shall be distinctive and reflective of their use. 'Franchise' architecture shall be avoided.
6. Where multiple buildings are constructed as part of a single-development, each building should appear unique and in harmony with the overall development. Monotony should be avoided.

B. Style

This Development Code does not dictate architectural style. Applicants are not required to select an architectural style from this section. Rather, applicants may choose to design buildings that draw from local building traditions, styles, and character, or may explore contemporary architectural styles in accordance with present-day building materials and construction methods, and in response to local climate and context.

The following guidelines are selected from the range of architectural styles that contribute to San Pablo's physical, social, and cultural heritage dating back almost 200 years. These guidelines are provided for reference only. Design creativity and innovation are strongly encouraged.



Distinctive columns and window treatment separate individual storefronts.

Early 20th Century Commercial

Common to commercial and mixed use building types.

1. A single simple volume or single dominant volume having additional wings of similarly shaped volumes. The ground floor may read as the building base articulated by large storefront windows, and often includes walls or columns in different materials than upper floors.
2. Roofs are typically flat, hipped or gabled. Well-detailed cornices and parapets are common.
3. Building facades are flat, often composed of brick, wood, or masonry work including pre-cast concrete.
4. Canvas or fabric awnings are common above storefront windows and above formal entries.
5. Upper story windows have vertical proportions. Transom windows are common above storefront and entrance doors.
6. Materials for walls include brick, wood, masonry, and stained wood. Base treatments often include concrete and stone plinths, and may be decorated with ornate colored tiles. Masonry block shall be avoided.
7. Arcades supporting upper story terraces are common.



Tile roofs, ornate grillework and deeply inset windows characterize Spanish and Mediterranean Revival buildings.

Mediterranean Revival

Common to all building types and uses, incorporates Mission, Spanish Colonial Revival, and Monterey architectural styles.

1. Horizontal building volumes accentuated with vertical towers, especially at corners.
2. Exterior walls are typically single plane expanses of smooth plaster wall (stucco) and may be articulated with ornament of stone or cast concrete.
3. Roofs are either flat or gabled, with overhangs to create deep shadow lines. Roofs may be clad in red or brown clay tiles.
4. Windows and doors are deeply recessed into the wall surface.
5. Windows are vertical in proportion and may be arched in key locations. Large windows should be multi-paned. Window groups may be separated by columns.
6. Double-hung and casement windows are common. Aluminum and/or white-vinyl should be avoided.
7. Building facades are typically pale shades of grey, white or flesh tones.
8. Stairs, balconies, terraces, and loggias may be accentuated with color tile, wrought iron, and lightly painted wood.

2.6 Building Design



Low-pitched, hipped, or gabled roofs may contain a front dormer.



Low-pitched, hipped, or gabled roofs may contain a front dormer.



Colonial Revival

Common to residential and civic buildings.

1. Rectangular simple volume, may be more square in the case of 'classic box' and more vertical for rowhouses.
2. Roofs are typically low-pitched hipped or gabled and may contain a front dormer. Wide, overhanging eaves are common.
3. Windows are vertically proportioned and may be adorned with sills, protruding trim, and shutters.
4. Windows are typically arranged symmetrically on building facades, and align between stories.
5. Doors are commonly accentuated with porticos and are centered on the primary building facade.
6. Walls are typically composed of horizontal wood siding, and may contain classical trim elements including pilasters and cornices.
7. Building entries may be enhanced with columns located symmetrically about the entry.
8. Classical detailing includes stained glass windows, pilasters, and columns.

California Contemporary

Common to all buildings types.

1. Simple accentuated rectilinear or square building volumes. Volumes may be more vertical (urban) for attached rowhouses and live-work buildings and more horizontal for commercial and workplace buildings.
2. Roofs may be either flat or pitched, shed, sloped or domed. Barrel vaults and gables may be asymmetrical.
3. Horizontal and vertical masses may be accentuated with extruded materials or contrasting colors.
4. Windows may be large and square compared with other more traditional styles, and may be stylistic and asymmetrical.
5. Exterior walls may include metal siding, giber cement siding, wood, stone, concrete, and stucco with a smooth finish.
6. Buildings may be accentuated with bold colors and use of untreated or industrial materials; Detailing is very minimal and rarely ornate.
7. Buildings should be unadorned, articulation should emphasize the expression of the structure.
8. Green roofs and solar panels are commonly incorporated into flat roofs.

C. Green Building Guidelines

Green Building Guidelines are strongly recommended as an integral part of building design and construction to conserve energy and material resources. The following categories are provided for consideration.

Energy Efficiency

1. Increased building performance may be achieved by using a well insulated building assembly, a tight building envelope, and energy efficient systems.
2. Buildings should be designed to maximize the use of abundant natural daylight as the primary source of illumination. Fenestration and building orientation should maximize heat gain in winter months and reduce gain in summer months.
3. Windows should be designed to maximize natural building heating and cooling where possible.
4. Roof overhangs, trellises or sunshades may be used to minimize solar heat gain.
5. Roof surfaces may be designed to incorporate solar panels. Photovoltaic and/or solar collectors shall be incorporated into overall building design and shall not appear as 'add-ons'.
6. Garden roofs may be used to provide insulating and energy benefits by reflecting solar radiation.

Materials and Indoor Air Quality

1. Materials that have low or zero volatile organic compounds (VOCs) and do not have other toxic chemicals that contribute to indoor air quality pollutants are encouraged.
2. Renewable durable materials should be used where possible for interior and exterior building finishes such as siding, flooring and carpet.
3. Operable windows should be used to provide fresh air circulation.



Landscaping provides shade on walkways.



Large roof overhangs minimize heat gain.



Green roofs may be incorporated in modern building designs.

Recycling & Reuse

1. Non-recyclable waste should be minimized during construction and renovation. Regulated waste clean-up methods are encouraged.
2. Recycled and/or salvaged building materials should be used whenever possible.

Stormwater Management

1. Buildings must direct stormwater to on-site infiltration flow-through planters, rain gardens, living roofs, and similar on-site collection and treatment systems.

Resources

- U.S. Environmental Protection Agency: Energy Star Program for Residences and Commercial Buildings. Program requirements include a combination of building envelope upgrades, high performance windows, efficient heating and cooling equipment, lighting, and appliances.
- The U.S. Green Building Council -LEED (Leadership in Energy and Environmental Design). Program includes guidelines and building certification that support sustainable design and green building practices.



Living roofs reduce energy costs and limit runoff.



Rainwater is directed to soils for 'living wall' trellis on commercial building in Emeryville, CA.



Window sunshades reduce solar heat and glare during summer months.

2.6 Building Design

2.6.3 Commercial and Mixed-Use Buildings

Articulation

Height

1. Ground floor storefronts are required along Chattleton Square East and West.
2. Ground-floor commercial uses should be a minimum of 14 feet clear interior height.
3. Buildings shall have a clearly defined ground floor delineated by a cornice, band, or other horizontal course. The ground floor shall be a minimum of 60% of the height of two-story buildings (measured to the second story ceiling height).
4. Horizontal courses such as watertables or bellybands or a change in facade materials are encouraged to help delineate upper stories where appropriate to overall style.
5. Roof treatment may include a cornice, parapet, eave or other distinctive treatment.



Formal entry provides access to upper story residential uses.



Residential lofts over ground floor retail and restaurants, front onto public square.



Corner tower with domed roof office above retail..

Length

1. Building length should be well articulated using facade elements in accordance with architectural style.
2. Building bays can be offset and roof lines varied to create a rhythm of building increments that provide visual interest and support pedestrian activity.
3. Where upper floors contain lofts, townhouses, flats or other residential units, length articulation should articulate and emphasize individual dwelling units.

Corner Treatment

1. Corner volumes may be accented by a change in materials, height, and/or use of a domed or pyramidal roof form.
2. Special windows and doors, terraces, and architectural materials are recommended methods for articulating corners.

Building Elements

1. Window glass shall be recessed a minimum of 2 inches from the exterior wall.
2. All windows with the exception of small accent windows and storefront windows shall be divided into a minimum of two panes.
3. Window glass shall be clear. Reflective glass windows are not permitted.
4. Alcoves, balconies and terraces are encouraged at upper stories to provide outdoor spaces.
5. Ornamental wall-mounted outdoor lighting is encouraged to accent entries, columns or pilasters.
6. Decorative elements such as tile mosaics, artwork, decorative reliefs, and ornamental lighting are encouraged to add vibrancy to building walls.
7. Blade signs and wall-mounted signs are encouraged to enhance pedestrian scale and character.



Significant architectural volume at corner creates a visual landmark along a major automobile corridor. Adjacent building bays designed in accordance with dwelling units.

Entries, Access, & Parking

Pedestrian

1. Residential entry doors facing the public realm shall have a minimum 10% glazing.
2. Commercial entry doors shall have a minimum of 50% door area glazing.
3. Upper story uses should be primarily accessed via formal entries. A lobby may provide access to an interior stairwell and/or elevator.
4. Recessed building entries may include special paving, soffit treatment, and decorative lighting.
5. Building entries shall be accentuated with pedestrian-scale elements such as canopies, overhangs, and awnings.



Varying building bay masses and roof forms enhance the gateway location of this mixed-use building.

Storefronts

Overview: Storefronts are required frontage conditions along the Gateway Boulevard and at key locations along Chattleton Square East and West. Design specifically for pedestrian activity-generating uses, storefronts shall be designed in harmony with overall architectural style.

Guidelines

1. Typical storefront design elements may include a bulkhead, window base, display windows, recessed entry, transom windows, frieze or sign band, and awnings or overhangs.
2. The primary storefront entrance shall be accessed directly from the public sidewalk.
3. Entrances may be recessed in a vestibule or alcove paved with decorative materials.
4. A minimum of 50% of the storefront facade shall be comprised of display windows having clear glazing. Opaque or reflective glazing is not permitted.
5. Awnings or architectural projections shall be located above all non-recessed doors and shall maintain 8 feet of vertical clearance from the sidewalk.
6. Portions of the storefront may be setback a maximum of 15 feet to create opportunities for outdoor seating and display of goods. Setback areas must be hardscaped and should receive a decorative paving treatment.
7. Awnings should span individual storefront bays. They should not span across building walls or pilasters. Awnings should consist of non-glossy materials including fabric, metal-framed glass and sheet-metal.
8. Decorative lighting of storefronts and signage is strongly encouraged.
9. Operable storefront doors should be located no more than 30' on-center. More than one storefront door may access a single tenant.



2.6 Building Design

2.6.4 Workplace Buildings



Formal entry and corner treatment emphasize corner location.



Individual roof forms articulate this two story workplace building.



Contemporary materials, massing and articulation.

Articulation

Height

1. Single story buildings should be designed as “grand” buildings, having an interior ceiling height of no less than 14 feet, and an exterior building height of no less than 20 feet.
2. Buildings should have a distinctive ground floor and roof treatment.
3. A base treatment is recommended. Base treatments should project a minimum of 1 inch from the wall surface and may employ a change in color, and/or change in material.
4. Additional height articulation with horizontal courses are encouraged to help delineate overall building height.
5. Ground floors should be taller than upper floors.

Length

1. Buildings length should be well articulated using facade elements such as columns or pilasters when appropriate to architectural style.
2. Building length may be articulated by offsetting building bays with corresponding roof forms.
3. Corner treatment using uniquely detailed roofs or volumes is encouraged to add visual interest.

Building Elements

1. Alcoves, balconies and terraces are encouraged at upper stories to provide outdoor spaces for employees.
2. Ornamental wall-mounted outdoor lighting or up-lighting of building elements is encouraged, adjacent to the public and private pedestrian areas.
3. Roof gardens or green roofs should be considered to enhance energy efficiency, reduce stormwater run-off, and provide visual interest.

Access & Parking

1. Primary access to ground floor tenants shall be from the fronting street.
2. Primary access to upper story uses shall be achieved through formal entries on front or side facades, or from a central lobby, stairwell or elevator.
3. Parking for workplace buildings should be primarily located in surface lots behind the primary structure, in nearby parking structures, and on-street.
4. Parking and services shall be accessed through an alley or City Street where possible.
5. A forecourt may provide an attractive space for public gathering adjacent to workplace buildings.

Note: Workplace buildings should not detract from walkability. New buildings should have a clear and significant street presence and employ methods of vertical and horizontal articulation, pedestrian-scale building elements, and welcoming frontage conditions along the primary street to support pedestrian comfort and encourage walking.



2.5 Building Design

2.6.5 Residential Buildings



Contemporary stacked flats with articulated building masses.



Individual roof forms articulate this two-story workplace building.



Row houses utilize stoops to enhance pedestrian activity.

Massing and Composition

1. Building massing should illustrate residential volumes through the offsetting bays, and use of individual roof forms and building elements in accordance with overall style.
2. Building facades shall be oriented to the primary street and to interior courts and paseos. Buildings shall not front onto parking areas.
3. Private outdoor spaces such as courtyards, plazas, and gardens are encouraged.
4. Ground floor residential units shall be elevated a minimum of 18 inches above finished grade.

Articulation

Height

1. Common roof treatment for stacked flats includes a cornice, parapet, cap or change in materials; for multiplex and rowhouses roofs may be pitched, include dormers, or a distinctive cap to provide visual interest in accordance with overall style. Modern buildings may include a combination of roof types to illustrate individual units.
2. Additional belt courses are encouraged to help delineate upper stories.

Length

1. Building masses should articulate individual units. Projecting and inseting of building volumes, delineation of bays, and varying roof lines should be employed to add visual interest.
2. Individual façade composition should not be excessively replicated. A variety of unit plans or façade designs should be employed over the length of a block face.
3. Corner treatment using unique roofs or volumes is encouraged to add visual interest to stacked flats.

Building Elements

1. For multifamily buildings, formal entrances may be identified with porticoes, vestibules, and prominent doorways. For rowhouses, stoops are recommended along street frontages.
2. Terraces, balconies, window bays, porches, and roof gardens should be used where appropriate to create outdoor rooms.
3. Hierarchy of window sizes should reflect the living spaces within.
4. Green roofs should be considered to enhance energy efficiency, reduce stormwater run-off, and provide visual interest.

5. Shading devices such as overhangs, latticework and trellises should be incorporated where appropriate, especially at south-facing facades.

Access & Parking

1. Primary pedestrian access to residential buildings shall be from the fronting street, paseos, and courtyards where possible. Entrances should be residential in character, be prominent, and easy to identify.
2. Upper story units should be accessed from a central lobby, or interior stairwell contained within the main building volume.
3. Parking should be in garages, structures or car ports accessed from City Streets and alleys where possible.
4. Garages, structures and car ports shall be designed in harmony with the primary building's architectural style, colors, and building materials.
5. Internal "tuckunder" garages may be accessed from rear alleys.
6. For live-work buildings, ground floor commercial spaces should be accessed directly from the primary street, court, or paseo where possible.

Recommended Building Types

Stacked flats

A multi-story building comprising flats, lofts, townhouses, or a mix of residential types arranged side-by-side and on multiple floors.



Live-work

A building designed for integrated residences and workspaces. Buildings may be organized as rowhouses (attached single-family) or may have multiple units in a single structure.



Multiplex

This building type appears as a large scale house and contains two, three, four, or more units which may be stacked or side-by-side. Common development types include duplex, triplex, quads, and villas. Carriage houses may be located at the rear of the property.



Rowhouse

A multi-story building comprising flats, lofts, townhouses, or a mix of residential types arranged side-by-side and on multiple floors.



2.5 Building Design

2.6.6 Parking Structures



Ground floor retail, unique building massing and roof lines, decorative grillework and vertical window openings.



Ground floor retail and vertical window elements 'break down' the horizontal nature of the parking structure building volume.



An arcade is appended to the ground floor to support pedestrian activity along the secondary facade of this parking structure.

Massing & Composition

1. A single distinctive volume with horizontally accentuated facades interrupted by vertical volumes and building elements.
2. Openings and bays should be vertically proportioned. Uninterrupted horizontal opening should be avoided.
3. Corner stair towers and accentuated corner volumes can be used to provide visual interest.
4. All facades visible from the public realm shall be designed with a high degree of architectural detail.

Articulation

Height

1. Buildings should have a distinctive ground floor treatment that complements the massing and articulation of adjacent buildings.
2. Special design of roof forms should be used to create an interesting 'skyline'. Roofs should include a parapet wall to block visibility of cars on the roof deck from adjacent public areas. A cornice or other treatment may be used for added detail.
3. Shade elements, cornices, trellises and grille-work should be incorporated to add visual interest.

4. When viewed from the public realm, parking decks shall not appear to ramp or jog at an inconsistent rate. Exterior wall design shall utilize decorative elements to 'mask' ramping floor decks.
5. Base treatments shall project a minimum of 1" from the wall surface and may include a change in color, or change in material, and should extend around all sides of the building.
6. Vertical elements such as pilasters or towers may extend beyond the roof line, but shall not exceed 60' in height.

Length

1. Storefronts (where permitted or required) are an excellent means of articulating the ground floor.
2. Building length should be well articulated using vertical facade elements appropriate to the building's style.
3. Corner treatment using unique roofs or volumes is encouraged to add visual interest.
4. Building length should be articulated using facade bays, pilasters, notches, roof forms or other architectural expressions at a maximum on-center distance of 30 feet.

Building Elements

1. Interior lights shall be recessed into ceilings, walls, columns, and other building elements, or shall include cut-off fixtures. Interior luminaires should not be visible from adjacent sidewalks.
2. Roof deck lighting shall be pedestrian scale. Utilitarian or 'cobra-head' lights shall be avoided.
3. Ornamental wall-mounted outdoor lighting (sconces) should be used to accent entries.
4. Additional lighting of columns and other architectural features is encouraged.
5. Ground floor facade openings should employ grille-work, trellises, or other decorative elements to mask interior spaces from public view.

Access and Entries

1. Pedestrian entrances should be highly visible, and treated with architectural elements such as decorative lighting and materials.
2. Pedestrian entrances should incorporate awnings or canopies to provide shade and shelter.
3. Vehicular entrances should be no wider than 25 feet and may be accentuated with attractive materials, grillework, lighting, and other ornamentation.

4. Exterior driveway surfaces shall be treated with attractive non-slip materials such as unit pavers or colored and scored concrete and shall be visually distinguishable from adjacent sidewalks to enhance pedestrian safety.
5. All parking structures shall incorporate dedicated bicycle parking areas adjacent to pedestrian entrances.

Stormwater Management

The top floor or 'roof deck' of a parking structure is essentially an elevated parking lot. Stormwater management should be an integral component of building design:

1. Integrate rainwater collection facilities such as cisterns or tanks within the building structure to collect and filter stormwater prior to releasing into the City's infrastructure.
2. Greenwalls are encouraged as a means of utilizing stormwater to enhance visual interest.
3. Downspouts should be connected to flow-through stormwater infiltration planters where possible.
4. Solar collectors and/or photovoltaic panels should be integrated into building design.



Parking structure with ground floor storefronts. Second story openings are vertically-proportioned and include decorative grille-work and shade devices. A combination of brick and stucco accentuates building articulation and creates an attractive setting in a pedestrian environment.



Vertically-proportioned openings mitigate the horizontal building volume and support pedestrian activity.



The auto entrance to this structure is nondescript, and demarcated with a "P" above the entry. The pedestrian entrance is treated with a formal entry and awning.



A retail anchor with prominent corner entry occupy the ground floor of this parking structure.



Brick base treatment in combination with glass and steel articulation enhance this parking structure's compatibility with adjacent building fabric.

Section 3: Implementation

- 3.1 Street Network**
- 3.2 Public Open Space**
- 3.3 Finance**
- 3.4 Administration**

3.1 Street Network

Contents

In accordance with the community's vision, this code requires the project area to be developed as a walkable network of streets, blocks, and public open spaces anchored by the centrally located Chattleton Green and Plaza.

- 3.1.1 Required Street Network- provides a description of the required street network including the location and guidelines for the development of each street.
- 3.1.2 Block F Street Network - includes guidelines for the development of Neighborhood Streets recommended as part of Block F development.
- 3.1.3 Required Street Types - includes standards and guidelines customized to guide the design of each required street type.

3.1.1 Required Street Network

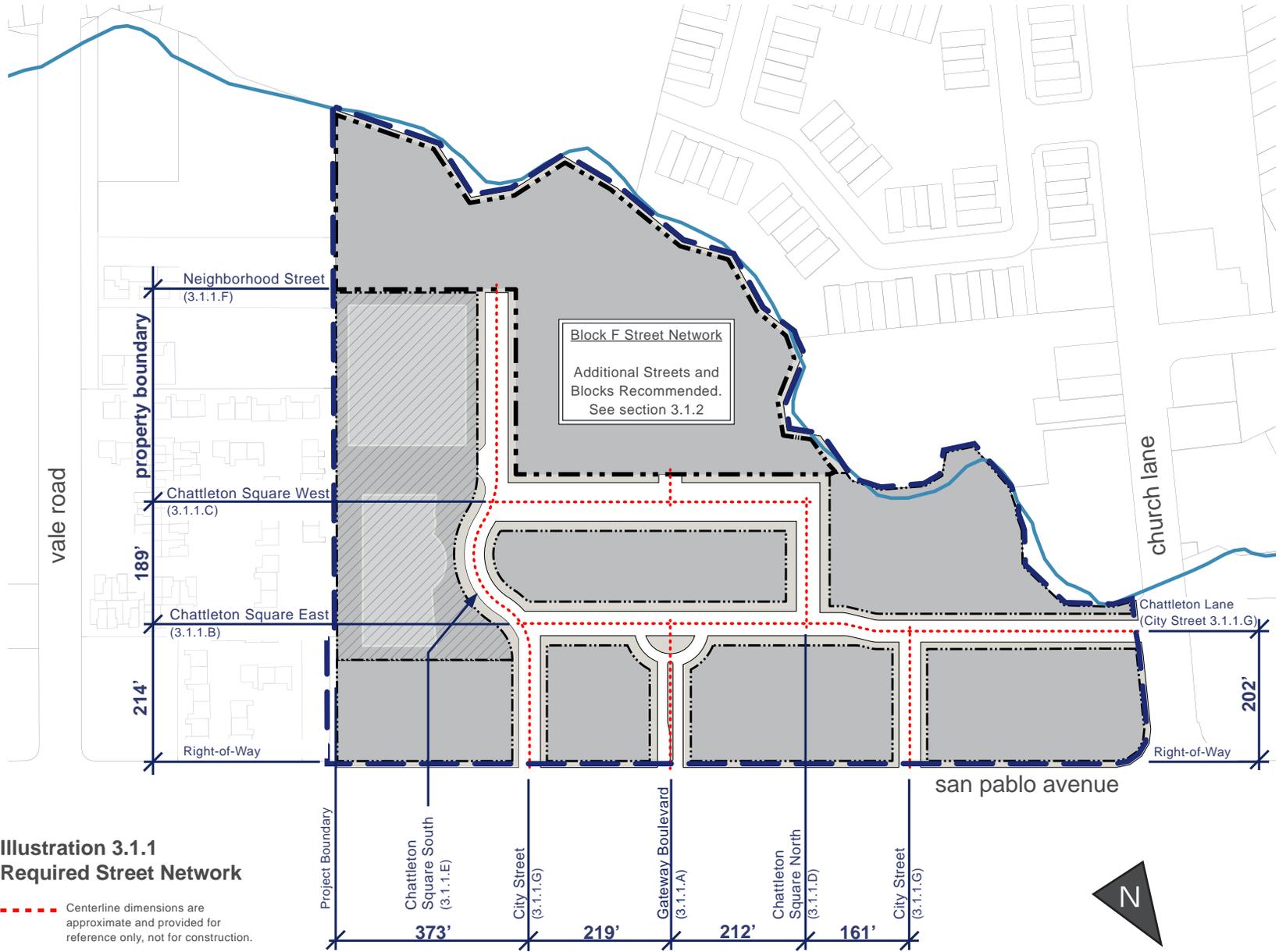
Illustration 3.1.1 on the following page locates the approximate centerline and limits for required streets throughout the Plan Area. On-center distances are based on the concept-level masterplan and are provided for reference only. At the time of implementation, street locations are likely to shift during the design development and construction documentation phase, as a result of survey information, utility requirements, and overall site engineering. Final layout of new streets shall be based on an accurate engineered site survey.

Required Street Types

- 3.1.3.A. Gateway Boulevard
- 3.1.3.B. Chattleton Square East
- 3.1.3.C. Chattleton Square West
- 3.1.3.D. Chattleton Square North
- 3.1.3.E. Chattleton Square South
- 3.1.3.F. Neighborhood Street & Boulevard
- 3.1.3.G. City Street
- 3.1.3.H. Alley

Exceptions

If an applicant wishes to propose significant modifications to required street design standards, they must demonstrate their design's capacity to enhance the pedestrian character of the streetscape environment and its compatibility with adjacent streets. All proposed modifications must be approved.



3.1 Street Network

3.1.2 Block F Street Network

Overview

In order to accommodate a range of development programming in accordance with investor preferences, this development code allows for a flexible approach to new streets in Block F, bound by Chattleton Green West and Wildcat Creek as shown in the adjacent diagram (3.1.2). Streets having the “Neighborhood Street” designation, including those adjacent to the West County Health Center parking structure and within Block F are designed to support a thriving residential or workplace district. To allow for greater flexibility new streets may be designed as a ‘boulevard’ or “street” as detailed in Section 3.1.3.D, and may include unique design features as detailed on the adjacent page.

Standards

1. A new Neighborhood Street is required to connect the required street network to the southern property boundary, in anticipation of a future connection to Vale Road. Location of connection to be identified as part of development proposal.

Guidelines

1. A “Neighborhood Street” or “Neighborhood Boulevard” may be constructed perpendicular to Chattleton Green West, with its centerline aligned with the centerline of the Gateway Boulevard, inviting the public to venture beyond Chattleton Square toward Wildcat Creek.
2. A new Neighborhood Street is recommended along the west side of the West County Health Center parking structure and may connect with the Street identified in number 1 above.
3. There is no minimum frontage coverage for Neighborhood Streets on Block F, and thus, new streets may serve as drive aisles within surface parking lots.
4. Pedestrian access between Neighborhood Street(s) and the Creekside Trail should be clearly identified.

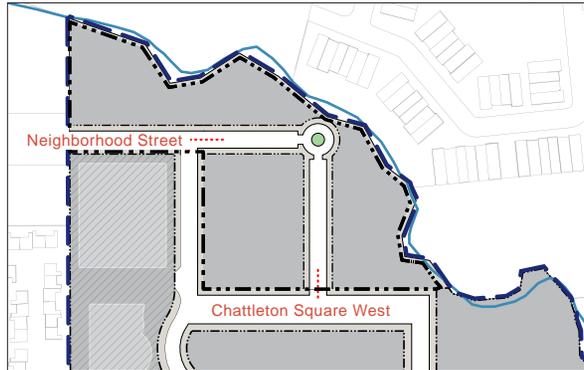
Suggestions

Three alternative street and block networks illustrated on the following page depict recommended alternative build-out scenarios that fulfill the vision for the Block F Street Network.



Illustration 3.1.2
Block F Recommended Street Connections

 Recommended Neighborhood Street point of connection

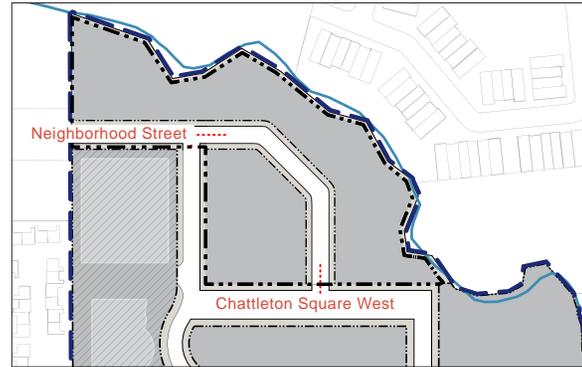


Alternative 1: Roundabout

Overview

A Neighborhood Boulevard extends west from Chattleton Square West, and couples with a Neighborhood Street extending from west of the WCHC parking structure via a roundabout.

The roundabout provides an opportunity to create a strong visual terminus to the east-west axis originating at the Gateway Boulevard and extending through Chattleton Plaza. A vertical expression such as an obelisk, clock tower, or formal planting is encouraged to create a focal point. On street parking is encouraged adjacent to access points to the Creekside Trail. (see Section 3.2.3)



Alternative 2: Deflected Vista

Overview

Neighborhood Streets extending from the suggested connection points are joined by an angled segment located approximately parallel to Wildcat Creek and the Creekside Trail.

Alternative 2 enhances opportunities to organize new development on both sides of new streets by maximizing the land available for new development between Neighborhood Streets and Wildcat Creek.



Alternative 3: Neighborhood Green

Overview

This street network alternative creates an intimate neighborhood green within a one-way 'loop' joining the recommended Neighborhood Streets. The green could serve as a valuable asset to nearby residential and/or workplace development, and could incorporate opportunities for a children's play area or community garden, while enhancing pedestrian access to the Creekside Trail.

3.1 Street Network

3.1.3 Required Street Types

Overview

New streets throughout the Plan Area should create a highly-attractive and pedestrian-friendly environment in support of the community's vision.

Objectives

1. Create a safe and attractive pedestrian environment to encourage and support walking throughout the Project Area.
2. Attract and support desired new investment by coordinating design details with the needs of nearby land use types.
3. Incorporate best practices for stormwater management including use of curb-inlets, permeable surfaces, infiltration planters and detention basins to provide on-site treatment and reduce run-off into storm drains.
4. Provide ample on-street parking stalls to support new development throughout the Plan Area.
5. Enhance bicycle ridership throughout the Plan Area and the City.

STANDARDS AND GUIDELINES FOR ALL PERMITTED STREETS

Paving Materials and Streetscape Framework

Standards

1. Permitted street types include Gateway Boulevard, Chattleton Square East, Chattleton Square West, Chattleton Square South, Neighborhood Streets and Boulevards, City Streets, and Alleys.
2. Streets shall be designed in accordance with Fire Department access requirements.
3. All travel lanes shall be paved using concrete, asphalt concrete, or concrete unit pavers.
4. Parking lanes may be constructed of asphalt concrete, or decorative paving materials including stamped and colored asphalt concrete, integrally colored and scored concrete, unit pavers, porous asphalt, pervious concrete, or turf-grids or bricks.
5. Sidewalks shall be constructed of integrally colored and scored concrete, concrete unit pavers, or bricks.
6. Crosswalks shall be clearly demarcated using striped painting, unit pavers, or a finished surface noticeably distinct from adjacent street paving. All crosswalks shall be ADA compliant.
7. Bulb-outs are required at all intersections, except along the Gateway Boulevard, to maximize pedestrian comfort, reduce crossing distances, and visibly narrow the travel way. Bulb-outs may include decorative paving materials to enhance pedestrian appeal.
8. The face of curb for all bulb-outs shall be set back from the edge of the adjacent travel lane a minimum of 18 inches and a maximum of 30 inches unless additional setback is necessary to accommodate emergency vehicles.
9. At intersections and bulb-outs, curb-return radii shall be minimized to reduce vehicle speeds and maximize pedestrian comfort.
10. Curb ramps are required at all intersections and crosswalks.
11. Loading and unloading zones shall be incorporated into new street designs where appropriate.
12. Streets shall be designed to be accessible to physically and visually impaired persons.

Guidelines

1. Parking lanes are encouraged to be paved with decorative paving to contrast them with adjacent travel lanes and provide an attractive ground treatment for pedestrian uses such as outdoor dining, food carts, farmers' markets, and other special events that may utilize parking lanes for pedestrian purposes.

Plant Materials and Site FurnishingsStandards

1. Street trees, with the exception of palms trees located along Gateway Boulevard, shall be deciduous and fast-growing, and shall be a minimum of 36" box size at installation.

Guidelines

1. In addition to required street trees, additional trees are encouraged to be planted within the parking lane (between on-street parking stalls) to increase shade, reduce heat glare, and increase overall pedestrian comfort in accordance with Section 12.16.050 of the SPMC.
2. Plants that have messy fruit or thorns should not be used adjacent to public sidewalks or open spaces.
3. Structural (engineered) soil should be used as a planting medium for all street trees with the exception of palm trees located along Gateway Boulevard.
4. Benches, bicycle racks, and decorative plant materials are encouraged within bulb-out areas and between street trees to enhance pedestrian comfort.
5. Site furnishings that support pedestrian comfort such as water fountains and news distribution boxes are encouraged.
6. Trash and recycling bins should be located at bulb-outs and at key pedestrian locations.

LightingStandards

1. Street lights throughout the project area shall incorporate cut-off and other "dark-sky" technology to limit light pollution.
2. Sodium vapor or other lights casting an orange glow are not permitted.
3. Streetlights shall consist of a decorative base, pole and luminaire; and maintain a consistent aesthetic throughout the project area.
4. All lights shall incorporate prismatic lenses, diffusers, or refractors to create soft and efficient distribution of light and avoid glare.
5. Area lights shall not cause glare or throw light onto upper stories of residential buildings.
6. Alleys, promenades, and pedestrian paths shall be well lit to increase safety and provide enhanced visibility.
7. Distance to the light-source for street lights and other pole mounted lights shall be between 11 and 14 above finished grade.

Guidelines

1. Bollard-mounted lights and in-ground lights are encouraged for promenades, pedestrian paths, and plazas.

3.1 Street Network

3.1.3.A Gateway Boulevard

Overview

The Gateway Boulevard serves as the 'front door' to the district. As the primary entry and exit corridor, the Gateway Boulevard's first responsibility is to communicate a high level of quality and character that invites visitors to venture within, and welcomes returning residents and workers.

The entry is framed with palm trees and arranged symmetrically about a planted median leading to the entry island. The median and entry island are provided to create a distinguished sense of arrival and help direct traffic. They are ideal settings for gateway elements such as custom wayfinding, signage and decorative streetlights.

A traffic signal shall be located at the intersection of San Pablo Avenue and the Gateway Boulevard. Extending west from the intersection, a single auto travel lane and a dedicated bicycle lane accommodates inbound traffic, and left and right turn lanes are provided for outbound auto traffic.

At the intersection of Chattleton Square East, the inbound travel lane is widened to accommodate both left and right turn movements. South of the entry island, a single lane of eastbound (outbound) traffic is provided for vehicles heading to San Pablo Avenue.

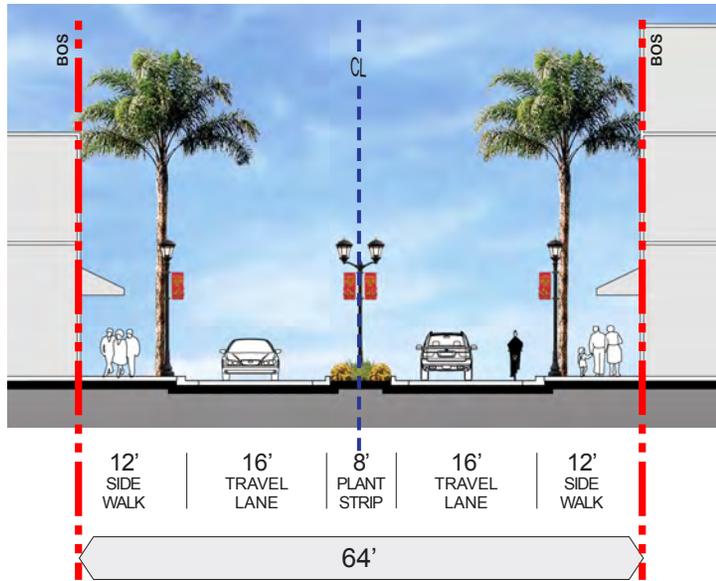
Standards

1. Inbound auto travel lanes (not including turning lanes) shall be 11 feet wide.
2. The inbound bicycle lane shall be 5 feet wide.
3. Turn lanes shall be 10 feet wide.
4. Sidewalks shall be 12 feet wide measured from face-of-curb to back-of-sidewalk.
5. Center median shall be a minimum of 4 feet wide (to accommodate outbound turn lanes) and a maximum of 8 feet wide, measured from curb face to curb face.
6. Plant materials within the center median and entry island shall be low lying and drought tolerant.
7. Palm trees shall be planted along sidewalks at the back-of-curb, maximum 30 feet on-center.
8. Scored and colored concrete or other paving materials shall be used to create a decorative 'surround' at the edges of palm tree wells.
9. Palm trees shall be a minimum of 20 trunk-feet at installation.
10. Streetlights shall be located at back-of-curb 30 feet on-center maximum, and coordinated with tree planting.
11. Streetlights shall include hardware for installation of banners.
12. Streetlights located within the center median shall have 2 or more luminaires.
13. At least 1 up-light shall be located at the base of each palm tree.

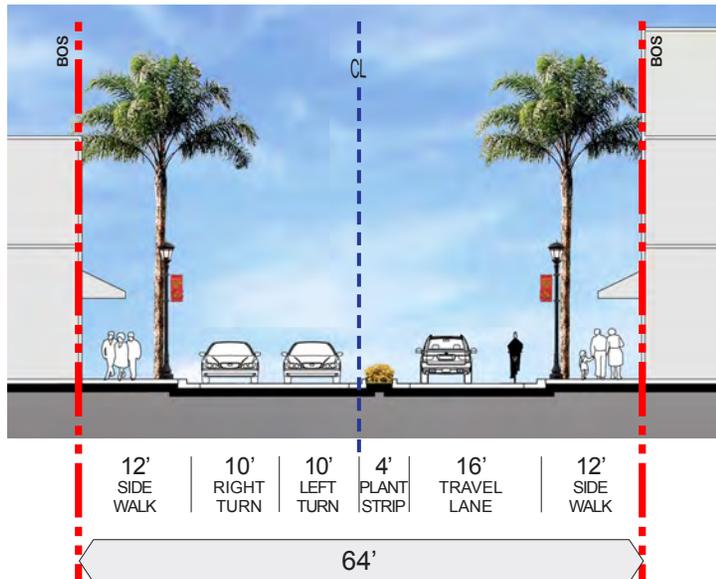
Guidelines

1. Decorative paving material are encouraged along sidewalks and crosswalks.
2. Recommended Palm Tree species includes:
 - Phoenix canariensis
 - Phoenix dactylifera

Section A-A'



Section B-B'



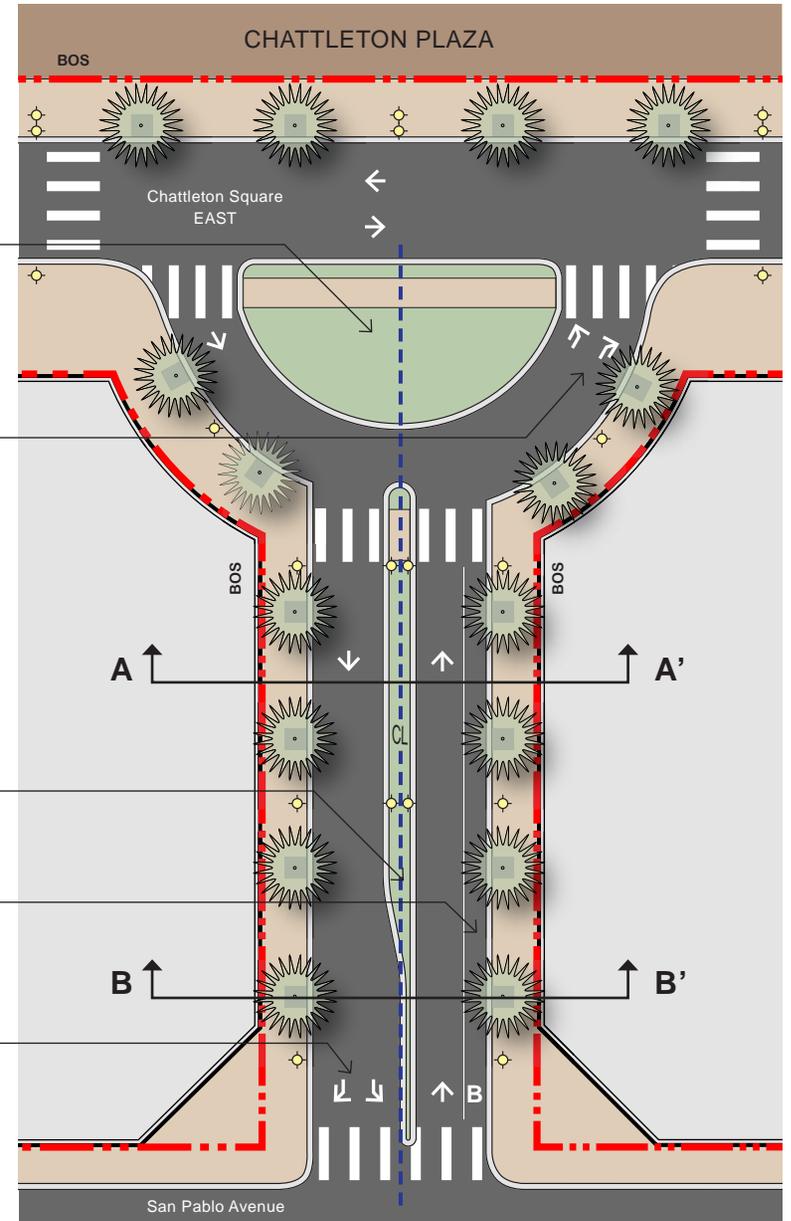
■ Entry island is ideal location for low lying directory signage

■ Right and left turn lanes

■ Center median

■ Bicycle lane

■ Right and left turn lanes



3.1 Street Network

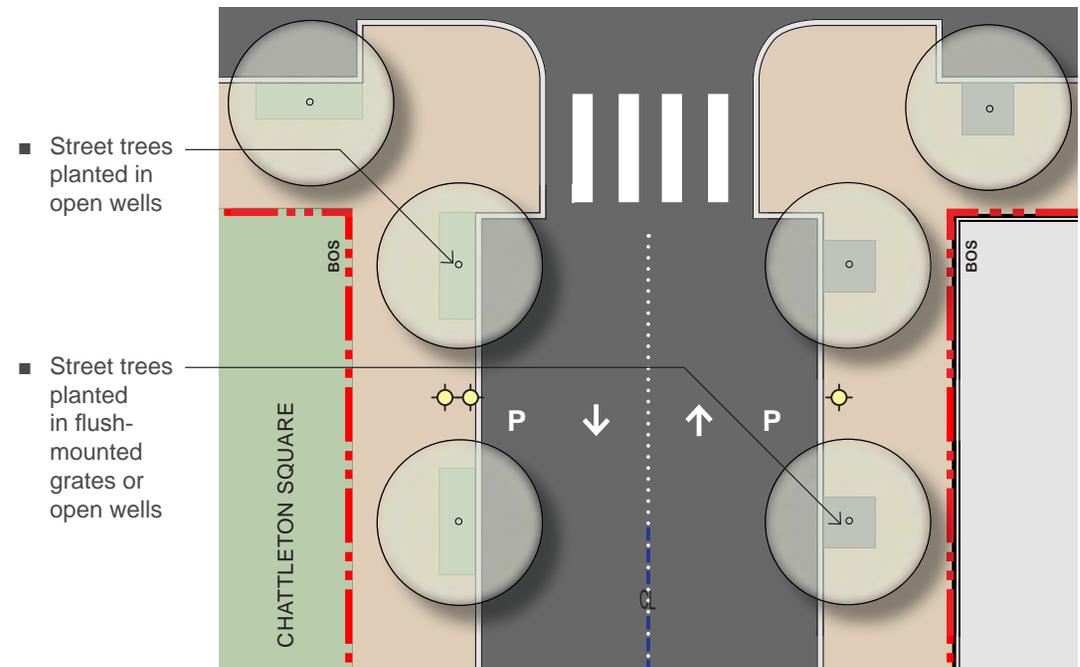
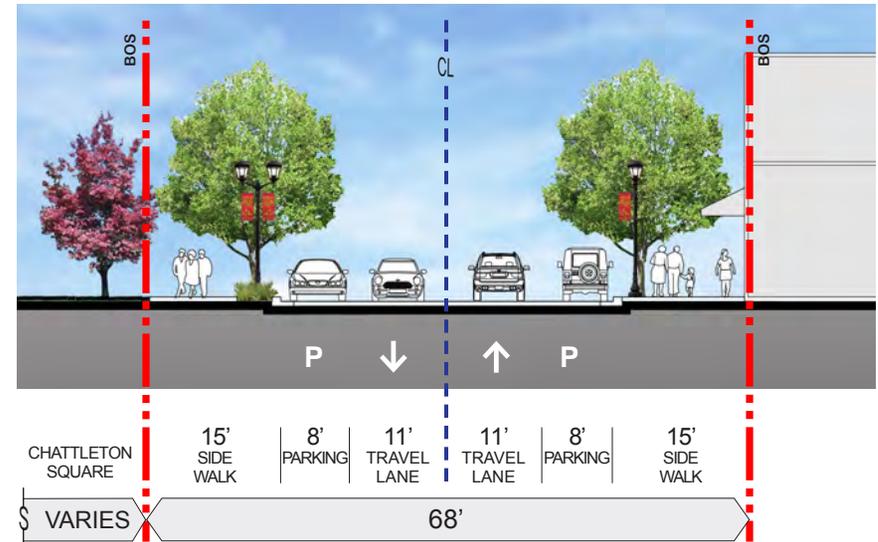
3.1.2.B Chattleton Square East

Overview

Chattleton Square East creates an attractive and highly-amenitized streetscape that supports pedestrian activity along the *eastern* side of Chattleton Square. Two travel lanes and parallel parking support ground-floor retail and commercial uses while large sidewalks encourage shopping and strolling and create outdoor space for outdoor dining, and the display of goods, adding value to ground floor tenants.

Standards

1. Travel lanes shall be 11 feet wide.
2. Parallel parking lanes shall be 8 feet wide.
3. Sidewalks shall be 14 feet wide from back-of-curb to back-of-sidewalk.
4. Top of curb shall be 1 foot wide.
5. A single species of street tree shall be planted maximum 30 feet on-center.
6. Trees on sidewalks along the east side of the street shall be planted in flush-mounted grates or planted wells, located at the back-of-curb, a minimum of 4 feet wide and between 6 feet long or within circular wells a minimum of 5 feet diameter. Open tree wells shall include a decorative surround comprised of unit pavers or colored and scored concrete.
7. Trees on sidewalks along the west side of street (adjacent to Chattleton Green and Plaza), shall be planted in wells located at the back-of-curb, a minimum of 4 feet wide and 8 feet long.
8. Where tree wells are utilized, a ground treatment such as crushed granite or low lying plant materials shall be used when planting substrate is exposed.
9. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.
10. Streetlights shall include hardware for installation of banners.



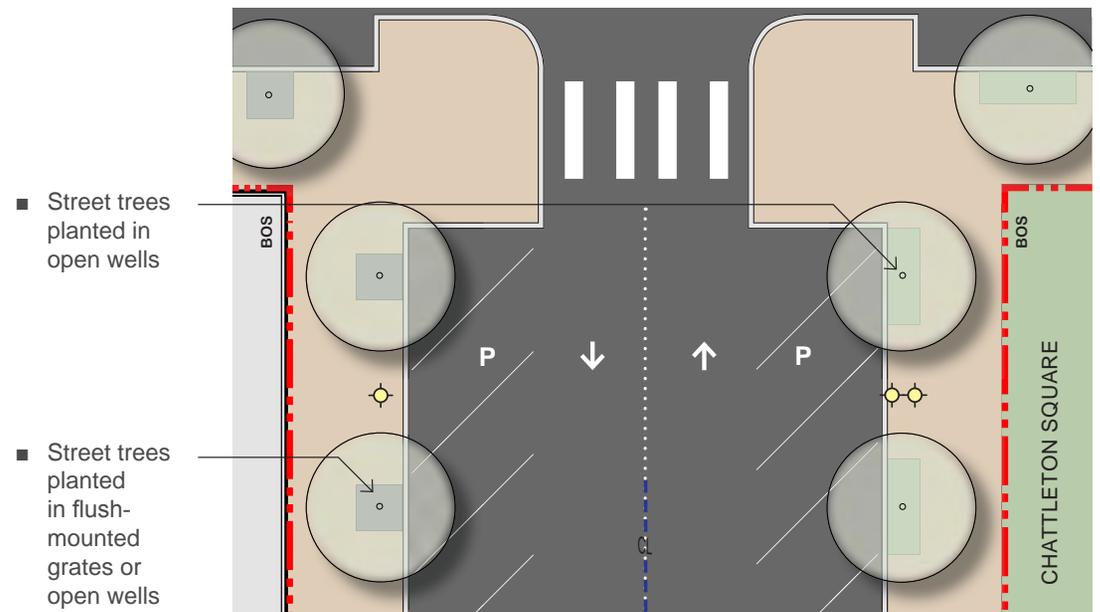
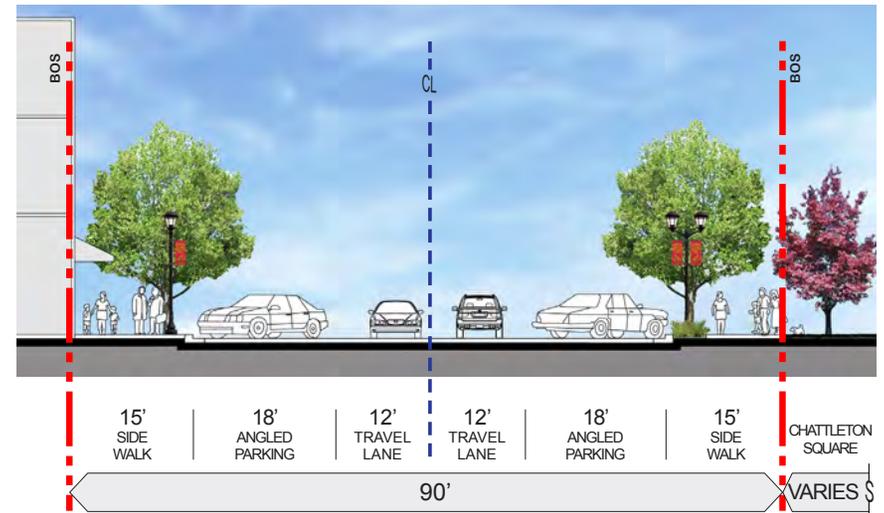
3.1.3.C Chattleton Square West

Overview

Chattleton Square West creates an attractive and highly-amenitized streetscape that supports pedestrian activity along the *western* side of Chattleton Square. Two travel lanes, an abundance of 45 degree angled parking, and generous sidewalks provide a beautiful setting that is complimentary to both commercial and residential uses.

Standards

1. Travel lanes shall be 12 feet wide.
2. 45 degree angled parking lanes shall be 18 feet wide.
3. Sidewalks shall be 14 feet wide from back-of-curb to back-of-sidewalk.
4. Top of curb shall be 1 foot wide.
5. A single species of street tree shall be planted maximum 30 feet on-center.
6. Trees on sidewalks along the east side of the street shall be planted in flush-mounted grates or planted wells, located at the back-of-curb, a minimum of 4 feet wide and 6 feet long or within circular wells a minimum of 5 feet diameter. Open tree wells shall include a decorative surround comprised of unit pavers or colored and scored concrete.
7. Trees on sidewalks along the west side of street (adjacent to Chattleton Green and Plaza), shall be planted in wells located at the back-of-curb, a minimum of 4 feet wide and 8 feet long.
8. Where tree wells are utilized, a ground treatment such as decomposed granite or additional low lying plant materials shall be used where planting substrate is exposed.
9. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.
10. Streetlights shall include hardware for installation of banners.



3.1 Street Network

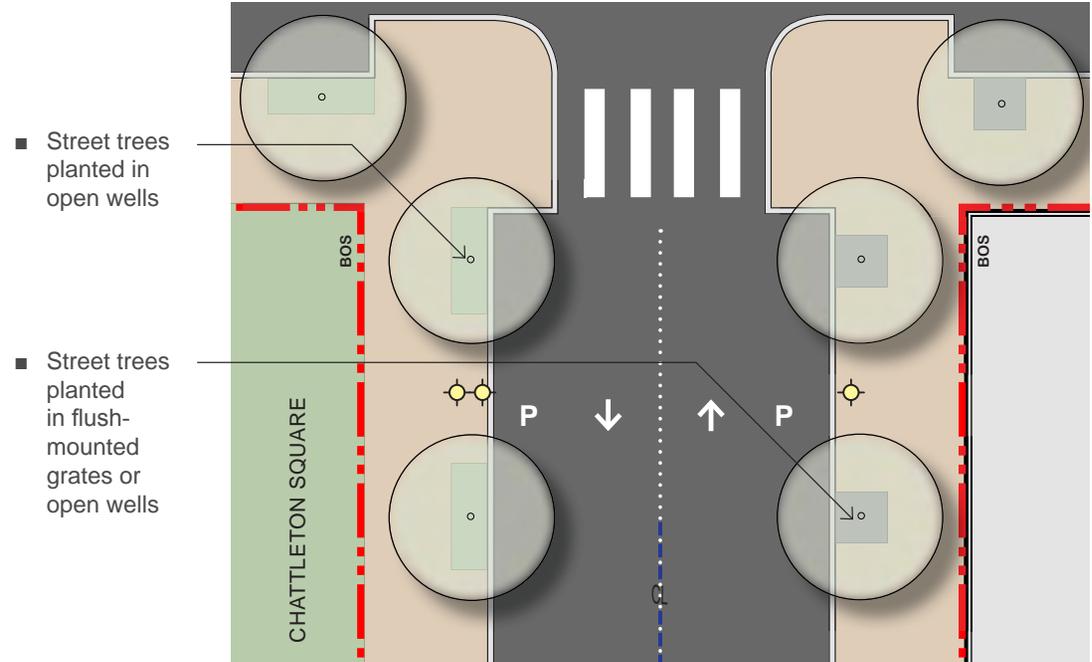
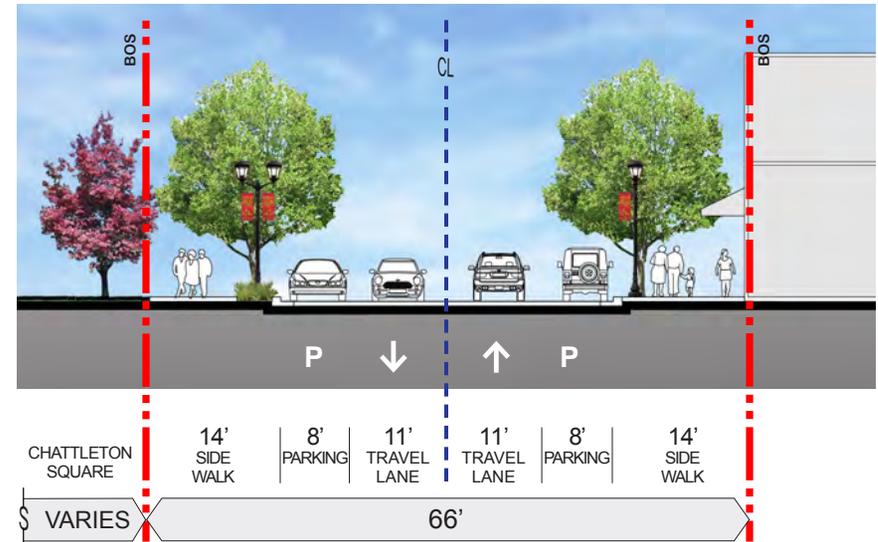
3.2.1.D Chattleton Square North

Overview

Chattleton Square North supports pedestrian activity adjacent to the cultural building located at the north end of the public open space. Two travel lanes and parallel parking support nearby land uses while creating a buffer to enhance pedestrian safety.

Standards

1. Travel lanes shall be 11 feet wide.
2. Parallel parking lanes shall be 8 feet wide.
3. Sidewalks along Chattleton Square shall be 14 feet wide from back-of-sidewalk to back-of-curb.
4. Sidewalks on the north side of the street shall be 11 feet from back-of-sidewalk to back-of-curb.
5. Top of curb shall be 1 foot wide.
6. Street trees shall be planted maximum 30 feet on-center.
7. On sidewalks adjacent to Chattleton Square (south side of street), street trees shall be planted in wells located at the back-of-curb, a minimum of 4 feet wide and 8 feet long.
8. On north side of street, street trees shall be planted in flush-mounted grates or open wells, located at the back-of-curb, 5 feet wide and between 5 and 8 feet long.
9. Where tree wells are utilized, a ground treatment such as crushed granite or low lying plant materials shall be used where planting substrate is exposed.
10. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.
11. Streetlights shall include hardware for installation of banners.



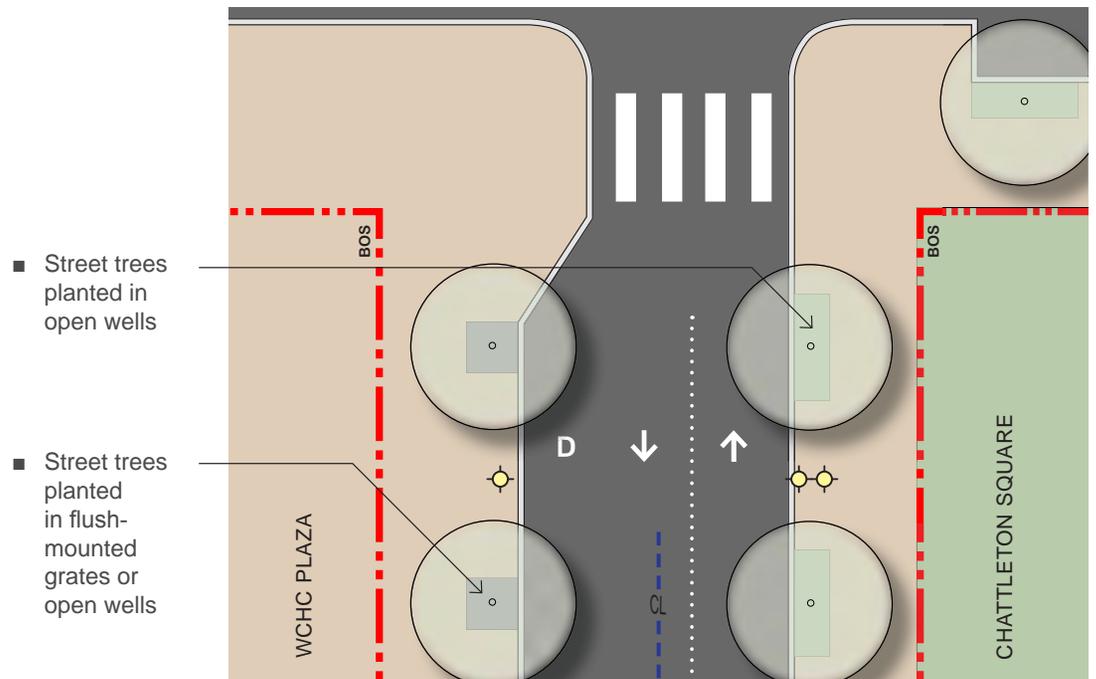
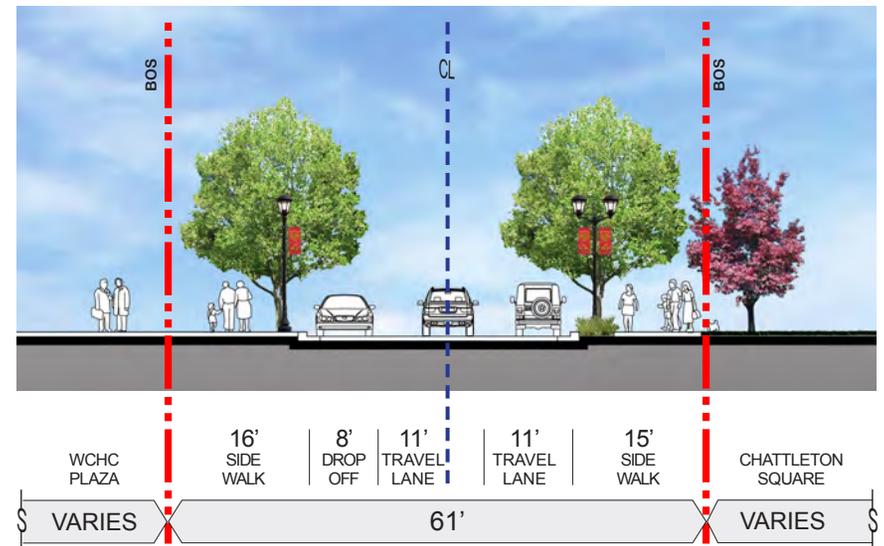
3.1.3.E Chattleton Square South

Overview

Chattleton Square South plays a critical role in the project area's street network adjacent to West County Health Center (WCHC) and Chattleton Green. A drop off lane along the southern side supports WCHC patron pick-up and drop-off, while allowing efficient vehicular movement around Chattleton Square. Streetscape design shall be refined and coordinated with WCHC site conditions.

Standards

1. Travel lanes shall be 11 feet wide.
2. WCHC drop off lane shall be 8 feet wide.
3. Sidewalk adjacent to WCHC plaza shall be 15 feet wide, and sidewalk adjacent to Chattleton Square shall be 14 feet wide; measured from back-of-curb to back-of-sidewalk.
4. Top of curb shall be 1 foot wide.
5. Street trees shall be planted maximum 30 feet apart on-center.
6. On sidewalks adjacent to Chattleton Square, street trees shall be planted in open wells, located at the back-of-curb, a minimum of 4 feet wide and 6 feet long.
7. On sidewalks adjacent to WCHC Plaza, street trees shall be planted in flush-mounted grates or open wells, located at the back-of-curb, a minimum of 4 feet wide and 6 feet long.
8. Streetlights shall be located at back-of-curb 45 feet on-center maximum spacing, and coordinated with tree planting.
9. Streetlights shall include hardware for installation of banners.
10. Where tree wells are utilized, planting areas shall be covered with mulch materials such as crushed granite or additional low lying plant materials.



3.1 Street Network

3.1.3.F Neighborhood Street

Overview

Street types having the Neighborhood Street designation, including those adjacent to the West County Health Center parking structure and within Block F (flexible block development as described in Section 3.1.2) are designed to support a thriving residential or workplace district. To allow for greater flexibility within the street and block network, Neighborhood Streets may be designed either as a typical street as detailed on this page, or as a 'boulevard' as detailed on the adjacent page.

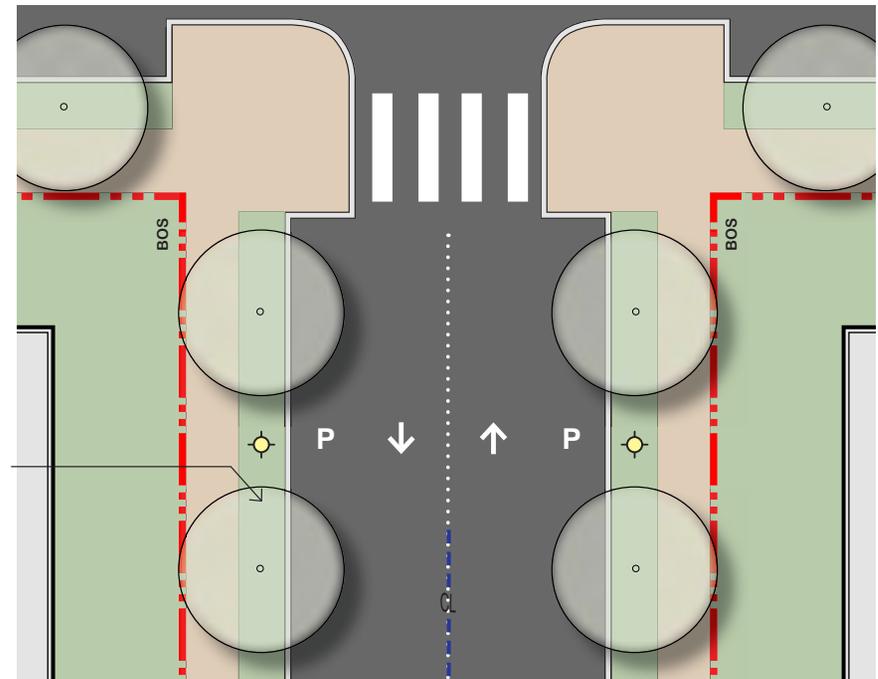
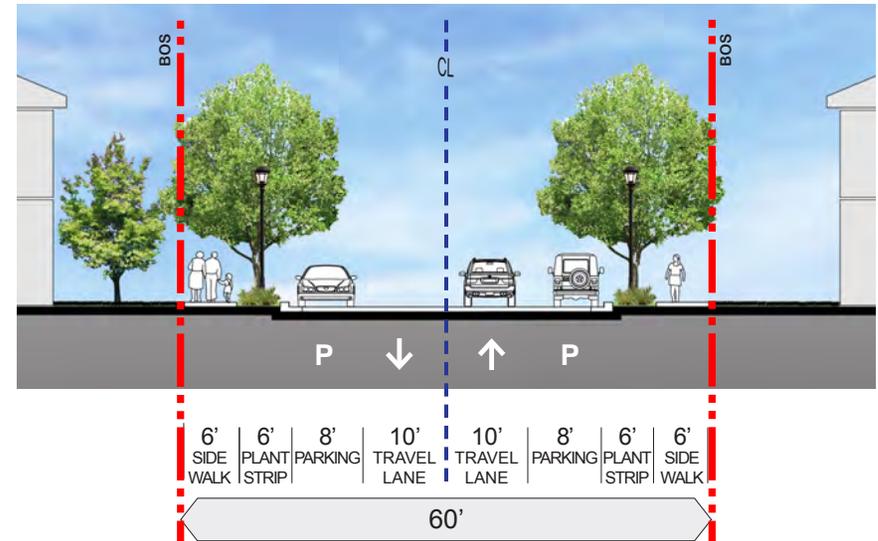
Neighborhood Streets consist of two travel lanes and adjacent parallel parking. Planting strips create a strong buffer and support pedestrian activity. Neighborhood Streets may incorporate roundabouts and one-way loops as detailed in Section 3.1.2

Standards

1. Travel lanes shall be 10 feet wide.
2. Parallel parking lanes shall be 8 feet wide.
3. Planting strips shall be located at the back-of-curb and shall be 6 feet wide.
4. Sidewalks shall be a minimum of 5' wide.
5. Breaks in planting strips shall occur at intervals no greater than 60 feet, to provide sidewalk access from parking lanes. Pathways connecting sidewalk to parking lanes shall be constructed of integrally colored and scored concrete, or unit pavers.
6. Street trees shall be planted maximum 30 feet apart on-center.
7. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.

Guidelines

1. Additional landscaping is encouraged within planting strips. Plant materials should be low lying and drought tolerant.



3.1.3.F Neighborhood Boulevard

Overview

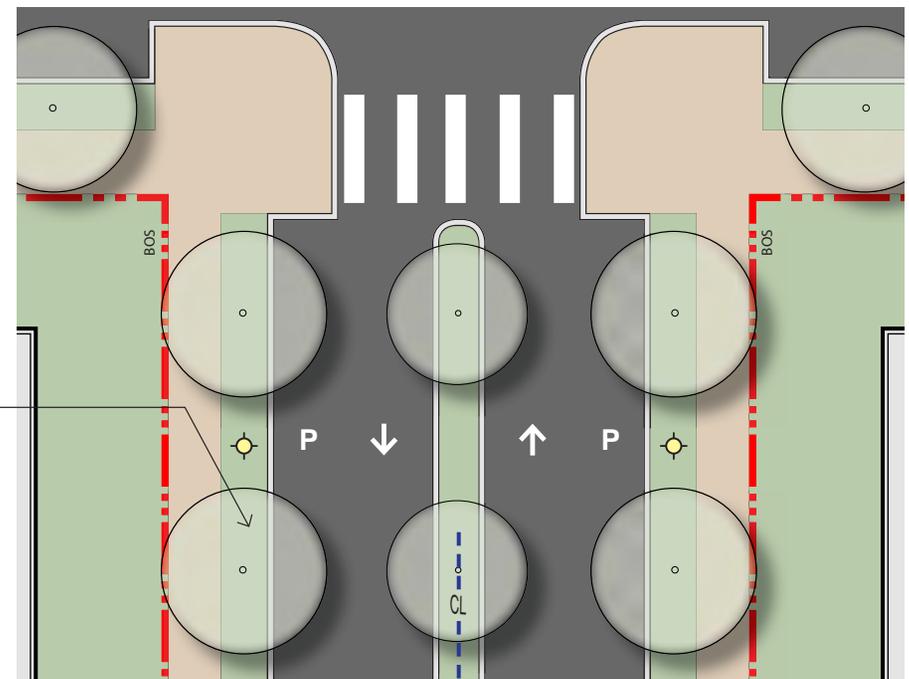
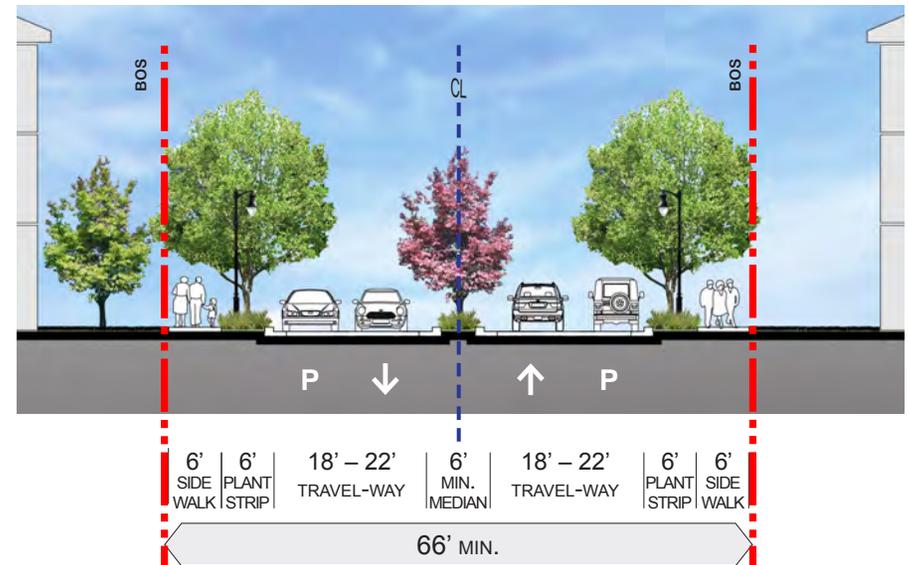
The Neighborhood Boulevard has two travel-ways, each consisting of a travel lane and parallel parking lane, arranged symmetrically about a planted center median. Adjacent to each row of parallel parking is a lush planting strip to provide an attractive streetscape environment for pedestrian activity, in support of residential or workplace development.

Standards

1. Boulevard shall have 2 travel-ways of consistent widths separated by a center median; a travel-way must include a travel lane and parallel parking lane.
2. Each travel-way shall be between 18 and 22 feet, measured from face-of-curb.
3. Center median shall have a minimum width of 6 feet, measured from face-of-curb; and contain street trees.
4. Planting strips shall be located at the back-of-curb and be a maximum of 6 feet wide.
5. Sidewalks shall be a minimum of 5' wide.
6. Breaks in planting strips shall occur at intervals no greater than 60 feet, to provide sidewalk access. Within breaks, paths shall be constructed of integrally colored and scored concrete, or unit pavers.
7. Street trees shall be planted maximum 30 feet apart on-center.
8. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.

Guidelines

1. Streetlights are encouraged within center median.
2. Additional landscaping is encouraged within planting strips and center median. Plant materials should be low lying, drought tolerant, and not have a negative impact upon the health of street trees.
3. Planting strip breaks should be provided to assist pedestrians moving between the parking lane and adjacent sidewalk.



3.1 Street Network

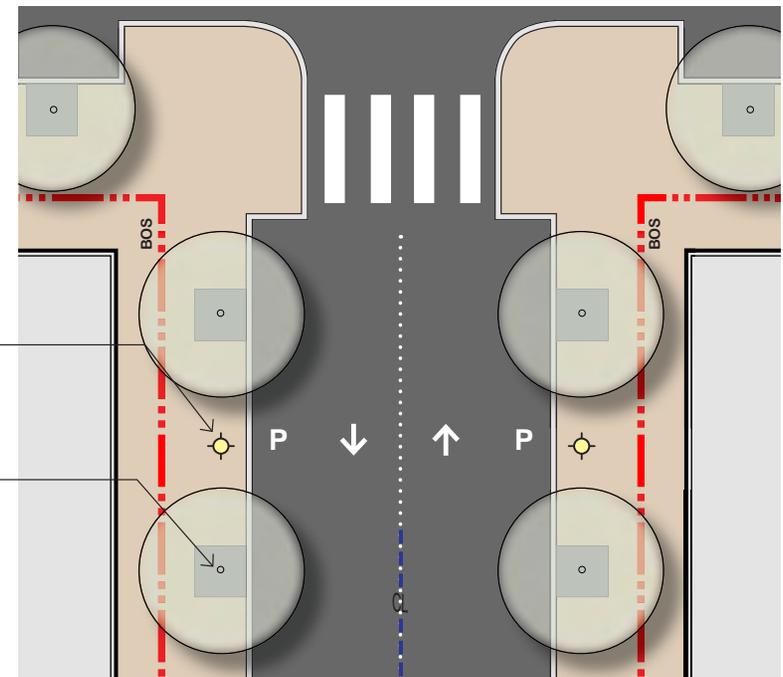
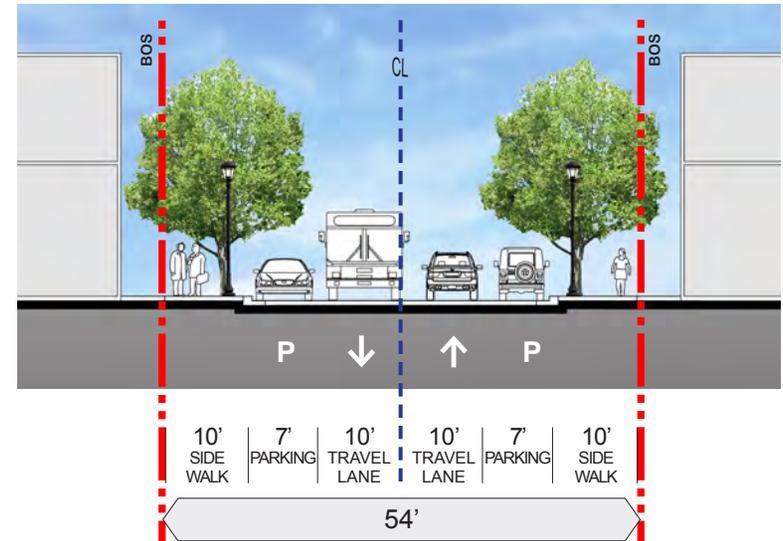
3.1.3.G City Street

Overview

City Streets act as important secondary access routes, connecting the Plan Area with the surrounding community. Relatively narrow, City Streets consist of two travel lanes and on-street parallel parking lanes and generous sidewalks and street trees to support a comfortable pedestrian environment.

Standards

1. Travel lanes shall be 10 feet wide.
2. Parallel parking lanes shall be 7 feet wide.
3. Sidewalks shall be 10 feet wide from back-of-sidewalk to curb face.
4. Street trees shall be planted maximum 30 feet on-center.
5. Street trees shall be planted in flush-mounted grates or open wells, located at the back-of-curb, a minimum of 4 feet wide and between 4 and 8 feet long.
6. Streetlights shall be located at back-of-curb 45 feet on-center maximum, and coordinated with tree planting.
7. Where open tree wells are utilized, a ground treatment such as crushed granite or low lying plant materials shall be used where planting substrate is exposed.
8. Overall street width shall comply with San Pablo street requirements.



- Streetlights are coordinated with street trees.
- Street trees planted in flush-mounted grates or open wells

3.1.3.H Alleys

Overview

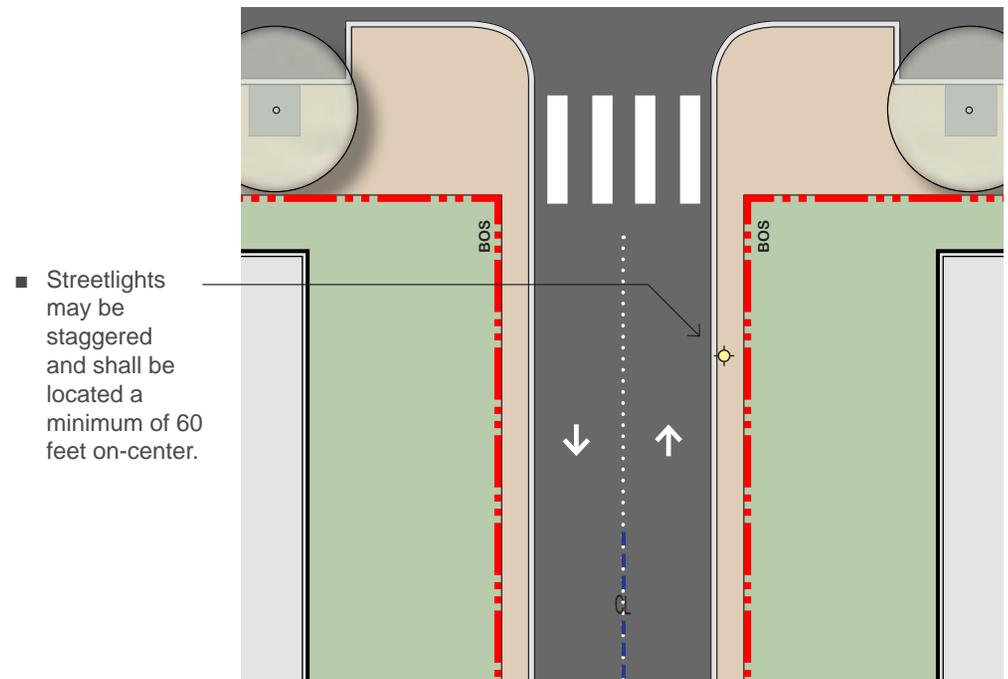
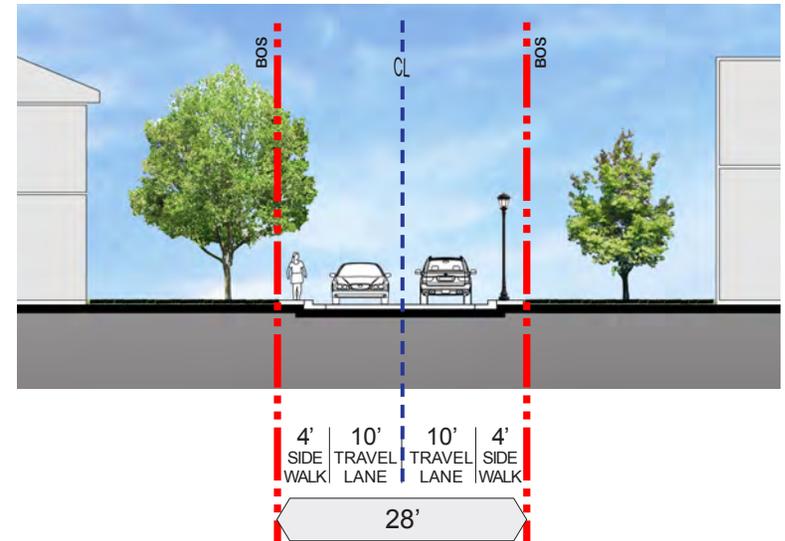
In addition to the project area's main street types, alleys shall be used to provide access to interior of blocks including garages, carriage houses, on-site parking, and services. Though serving primarily as access routes, alleys shall be safe and pleasing for pedestrians. The final location for alleys and access lanes shall be determined as part of a development application.

Standards

1. Alleys shall accommodate two-way traffic.
2. Travel way shall be a minimum of 18 feet and maximum of 24 feet wide.
3. On street parking is not required.
4. Sidewalks shall be minimum 4 feet wide and provided along a minimum of one side of the travel way.
5. At least 1 streetlight shall be provided every 60 feet.

Guidelines

1. Permeable paving materials and/or unit pavers are strongly encouraged within alleys.



3.2 Public Open Space

Overview

This plan requires the development of a series of public open spaces to create a distinctive setting for desired new investment, and to support the Plan Area's role as a unique local and regional destination.

Contents

3.2.1 Chattleton Plaza

An overview, development standards and design guidelines are provided to guide the development of the hardscaped community plaza and performance space.

3.2.2 Chattleton Green

An overview and design guidelines are provided to guide the development of the central public green.

3.2.3 Creekside Trail

An overview, design standards and guidelines are provided to ensure that new development fulfills the community's vision for a multi-purpose trail adjacent to Wildcat Creek throughout the Plan Area.



Illustration 3.2
Required Public Open Spaces

3.2.1. Chattleton Plaza

Overview

The Plaza at Chattleton Square plays a vital role as the heart of the district. Centrally located within the project area and on axis with the Gateway Boulevard, the highly visible plaza will communicate its prominence through a high level of quality and character. With a new cultural building located along the plaza's northern edge and a central water feature as its focal point, the plaza provides an attractive setting for outdoor performances, outdoor dining, art shows, exhibits, and serves as an ideal location for members of the community to participate in civic and cultural events during the daytime and evening alike. The Plaza may include a memorial honoring local Native American tribes.

Standards

1. The square Chattleton Plaza shall be approximately 110 on a side, centered on axis with the Gateway Boulevard.
2. A minimum of 80% of the plaza's surface shall be hardscaped and passable.
3. Special paving treatment such as colored concrete with a unique scoring pattern, unit pavers, or 'stamped' and colored asphalt concrete shall be used to distinguish the plaza as a high-quality pedestrian area.
4. A significant water feature such as a custom fountain shall accessible located in its center
5. A minimum of four Palm trees shall be located along the east and west edges of the Plaza.
6. At least 1 up-light shall be located at the base of each palm tree
7. Views from center of plaza to adjacent cultural building shall not be obstructed by plants or site furnishings.
8. The Plaza shall be well lit to increase safety enhanced visibility.
9. Height to the light-source for all lights shall not exceed 14 feet above finished grade

Guidelines

1. Bollard, pole-mounted, and in-ground lights are encouraged within the Plaza to create safe and attractive spaces.
2. Seating and other site furnishings, such as planters, sculptures, and tables, which support pedestrian comfort are encouraged.
3. In addition to the required Palm Trees, deciduous trees and/or decorative plant materials may be included to increase shade, reduce heat glare, and increase overall pedestrian comfort.
4. The plaza should be able to accommodate temporary kiosks and during festivals and events. Auxiliary power outlets should be located provide power for public events and performances.



Artist's rendering of Chattleton Plaza illustrating central water feature and decorative planting.



Children play in water jets, Tracy, CA.



Custom water feature, Redwood City, CA



Chattleton Plaza will host civic gatherings and performances.



Moveable tables and chairs beneath deciduous trees.

3.2 Public Open Space

3.2.2. Chattleton Green

Overview

Chattleton Green extends from Chattleton Plaza to Chattleton Square South, providing the community with ample space for active and passive recreation. The green functions as the San Pablo community's 'great lawn', giving local residents and workers a beautifully landscaped environment in which to congregate as a community, to visit as individuals as part of the work day, or for a Sunday morning coffee and stroll.

Guidelines

1. The turf area of the Green shall be approximately 80 feet in width, and extend from the south edge of Chattleton Plaza to the back of sidewalk adjacent to Chattleton Square South.
2. Large deciduous trees should be located along the perimeter of the Green at a maximum distance of 30 feet on-center
3. Deciduous trees and other plants with high ornamental value are encouraged to increase shade and overall pedestrian comfort but shall not obstruct views between the Green and Chattleton Plaza.
4. A minimum of 60% of the green's surface should be turf grass to support a range of activities and events.
5. Community amenities such as a play area for young children, ornamental gardens, public restroom facilities, and installation of public art is encouraged, but should not exceed more than 20% of the total area.
6. Seating and other fixed site furnishings such as planters and tables, trash and recycle bins, water fountains and other features that support pedestrian comfort are encouraged.



Vast unobstructed open space for relaxing.



Chattleton Green shall serve as the community's primary space to relax and spend time in an attractive outdoor environment. The 'Green' can also serve as a focal point for formal gatherings and large scale community events, farmers' markets, exhibits and performances.



Farmers' market stands locate within the parking lane along perimeter of Community Green, Madison, Wi.

3.2.3 Creekside Trail

Overview

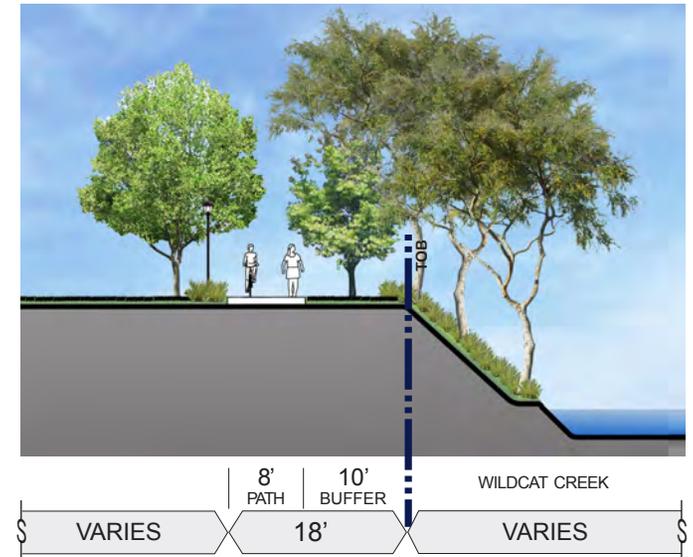
The multi-purpose Creekside Trail provides a peaceful pathway for pedestrians and workers to bicycle and stroll alongside Wildcat Creek throughout the Plan Area, and serves as a Bay-to-Ridge Trail connector, providing access to the Bay and to regional parklands. An access point near the intersection of Chattleton Square North and Chattleton Square West connect the Trail to the heart of the project area. A second access point located near the western edge of the West County Health Center parking structure allows for future connection via a bridge to Salesian High School and nearby neighborhoods.

Standards

Location, Access, Furnishings, and Materials

1. The Creekside Trail shall be designed in accordance with C.3. Stormwater requirements.
2. Access points shall be located near the intersection of Chattleton Square North and Chattleton Square West to connect the Trail to the heart of Chattleton Square, and near the intersection of Neighborhood Streets adjacent to the WCHC parking structure.
3. A minimum 10 foot buffer is required between the top of creek bank (TOB) and Creekside Trail. The final buffer setback shall be established in coordination with the Public Works Department.
4. The Trail shall be paved with integrally colored concrete or unit pavers.
4. Bollard or pole-mounted lighting shall be located along the trail at a maximum distance of 80 feet on-center. Height to the light-source for pole mounted lights shall be a maximum of 12 feet above finished grade.
5. Seating areas shall be provided at required access points.
6. Seating areas shall include a bench or custom seat, pole-mounted light, and a security call box.

Trail Conditions (typical)



Trail Conditions (access points)

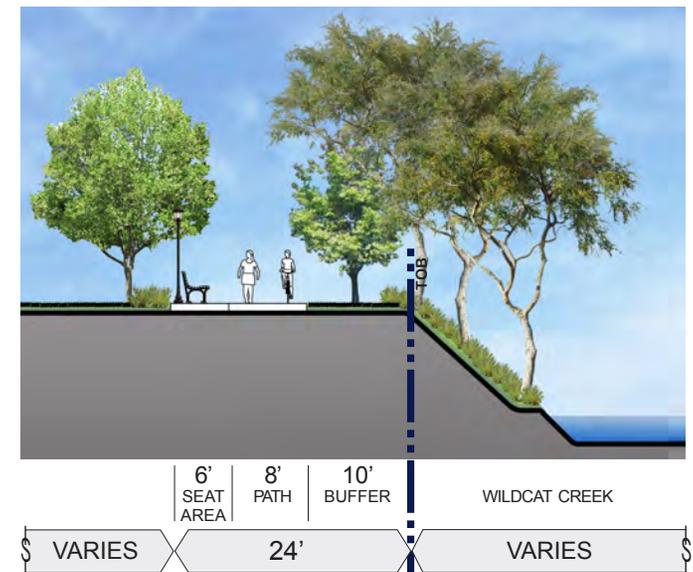


Illustration 3.2.3
Creekside Trail Limits
and Points of Access